

2014

CARS



General Competition Rules and Regulations



Canadian Association of Rallysport



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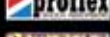
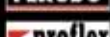
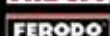
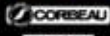


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Canadian Association of Rallysport

CARS Rally GCRs

and Rally Regulations

2014

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These regulations are intended to assist in the conduct of competitions and to further general safety. They are a guide and in no way guarantee against injury or death to participants, spectators or others. No express or implied warranties of safety or fitness for a particular purpose shall be intended or result from publication or compliance with these regulations.

By participating in these events, all participants are deemed to have agreed to be bound by these GCRs and Regulations.

Copies of this book are available from the CARS Office for **\$20.00** CDN per copy.

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Publishing Date: January, 2014

LEGEND

Revisions and additions made since the last edition are shown thusly. Note the bar in the margin.

Deletions made since the last edition are shown thusly. The text has been left in place for your information only and will not appear in the next issue.

Canadian Association of Rallysport

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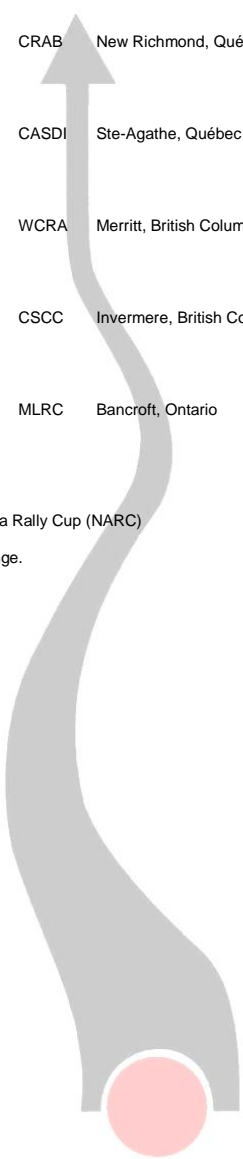

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Seed List/ Lifetime Standings:	George Dewar	seeding@carsrally.ca
Translator:	Pierre Racine	translator@carsrally.ca

Canadian Association of Rallysport

2014 Canadian Rally Championship



February 7 - 9	Rallye Perce-Neige Maniwaki www.rallyperceneige.com Patrick Rainville 819-663-5546 info@rallyperceneige.com	CRPN	Maniwaki, Québec
June 28 - 29	Rallye Baie des Chaleurs www.rallyebdc.com Jean-François Guité 418-751-4174 info@rallyebdc.com	CRAB	New Richmond, Québec
September 5 - 6	Rallye Défi www.rallyedefi.com B.Gilles Lacroix 450-475-8828 info@rallyedefi.com	CASDI	Ste-Agathe, Québec
October 3 - 4	Pacific Forrest Rally www.pacificforestrally.com Paul Westwick 604-617-4132 paul@rallybc.com	WCRA	Merritt, British Columbia
October 31 - November 1	Rocky Mountain Rally www.rockymountainrally.com John Blaber 403-671-1985 john@rockymountainrally.com	CSCC	Invermere, British Columbia
November 28 - 29	Rally of the Tall Pines www.tallpinesrally.com Ian Wright 416-606-5141 ian@tallpinesrally.com	MLRC	Bancroft, Ontario

All CRC events are also part of the North America Rally Cup (NARC)

This schedule is subject to change.

Cars Code of Conduct

All CARS clubs' members and all participants in CARS events shall conduct themselves according to the highest standards of behaviour and sportsmanship and in a manner that shall not be prejudicial to the interests and the reputation of CARS or its Regions or of motorsport generally. Failure to do so shall be deemed a breach of the CARS GCRs and may result in penalties being applied.

The purpose of this document is to provide a clear statement of the beliefs, expectations and ideals of CARS. In addition to any precepts provided in CARS Rules and Regulations, Bylaws or other Policies or those of its Regions, the following specific statements further delineate principles of individual conduct that CARS believes should be exemplified:

1. All participants in CARS sanctioned activities shall be bound by this Code of Conduct;
2. All participants in CARS sanctioned activities shall accept that motorsport can be dangerous and entails inherent risks;
3. All participants in CARS sanctioned activities shall be treated with consideration and respect;
4. All participants in CARS sanctioned activities shall endeavor to portray a positive image of rallying through their exemplary driving habits;
5. All competitors in CARS sanctioned events shall assure that their vehicle complies with the regulations of the event and the class that it is registered for and that their vehicle is presented in roadworthy condition;
6. Participants in CARS sanctioned events shall not knowingly place themselves or others in a position of undue risk. Consideration of safety shall be placed before competitive goals;
7. CARS members shall pledge to demonstrate in their actions care and concern for the environment;
8. All CARS members shall strive to set exemplary standards of behaviour as they are all ambassadors for rallying.

Rule Change Guidelines

General Outlines:

A rule change may be proposed at any time throughout the year and may come from someone in the sport, the rule change committee, an organizer, a CARS official or the CARS Board.

Once a rule change proposal is made, the proposal will be reviewed by the rule change committee under the direction of the CARS Technical Director. The proposal will go through various levels of discussion, translations and inputs from various groups which are stakeholders in our sport before the Board of Directors makes a final decision on the proposal. If the proposal is accepted, the rule change will likely take effect the following calendar year unless i) a CARS bulletin is issued or ii) a later effective date is attached to give the sport a sufficient lead time to prepare for the change in the regulations.

Process for Submission:

1. The proposal, in either English or French, should be sent via email to the Technical Director and CARS Office.
2. Your proposal should be worded as it should appear in the published regulations complete with regulation number.
3. The proposal should include a concise rationale outlining the reasons why the rule change is needed.
4. All rule change proposals should be dated and include the name of the person making the rule change proposal.

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1. GENERAL PRINCIPLES**1.1 National Control of Competition**

CARS is affiliated to ASN Canada FIA, holder of Canada's national sporting powers by delegated by the International Automobile Federation (FIA). By signed agreement, CARS has been awarded the sporting powers for Rallysport in Canada. ASN Canada FIA has retained the sporting powers for international level rallysport in Canada.

Since CARS is the governing body of Rallysport in Canada, CARS shall authorise, sanction and supervise all rallysport activities and do any and all things which "in its judgement" are conducive to the well-being and conduct of rallysport events held under the permit of CARS.

1.2 CARS Rally General Competition Rules

The CARS Board has established these Rally General Competition Rules, hereafter referred to as the Rally GCRs or GCRs.

1.3 Interpretation of Rules

CARS shall be empowered to decide any question raised within Canada concerning the interpretation of these rules subject to the right of appeal (if any) under these GCRs.

1.4 Exercise of the Powers of CARS

Judicial powers and functions, such as the hearing of protests, appeals, a disciplinary hearing or a CARS ordered investigation, and including the penalty to be inflicted for a breach of the CARS Code of Conduct, a breach of these GCRs or as provided for in the sporting regulations of CARS or its regions may be exercised by the CARS Board, a CARS region, a Steward, a Stewarding group or other group (whose number shall be not less than three) so appointed. The decisions of such groups shall be final and not subject to review except on appeal of Stewards' decision as a result of a protest hearing. No member of a judicial board shall have taken part, except as a Steward, in the competition concerned about which a decision is to be given or if he/she has already participated in a decision on the matter in question or if he/she is directly or indirectly concerned in such matter.

1.5 Notices

Any communications required under these GCRs to be sent to any entrant or competitor shall be sent to the address on the entry form or, if the competitor is the holder of a licence issued by CARS to the address on the licence. Any communications to be sent to a Promoter or Organiser shall be sent to the address on the relevant application for an Organising Permit. Any communications sent to a club shall be sent to the Secretary of the Organising Club at the address given in the club's membership application. Any communications so sent by signed post shall be deemed to have reached the appellant.

1.6 Alteration of Rules and Regulations

The CARS Board reserves to itself the right at any time and from time to time to alter its Rally General Competition Rules or its National Rally Regulations or to promulgate special rules in the form of a bulletin.

Such alterations or additions will be published in the form of revised manuals or bulletins. Bulletins issued by CARS form a part of these GCRs or the CARS National Rally Regulations to which they apply and are effective on the date issued until issuance of a new rule book unless amended or revoked earlier by CARS.

1.7 Regulations

CARS regions may publish regulations from time to time governing the conduct of rallysport events in their region. These regulations are considered in addition to and may in no way conflict with these GCRs.

1.8 Application of these GCRs

These GCRs, the National Rally Regulations and applicable Region Rally Regulations shall govern all Performance Rallies sanctioned by CARS. These GCRs and Region Rally Regulations shall govern all Navigational Rallies sanctioned by CARS.

1.9 Order of Precedence in Application of Rules and Regulations

Section 1.8 above specifies which regulations apply to the various types of rallies; the following defines the order of precedence of these rules and regulations.

- (a) These Rally General Competition Rules;
- (b) National Rally Regulations;
- (c) CARS Bulletins;

- (d) Regional Rally Regulations;
- (e) Series Supplementary Regulations (if applicable);
- (f) Event Supplementary Regulations.

Where conflict arises between these documents, the superior (first mentioned) document shown in the above list shall apply, except that bulletins may modify superior documents. With the approval of both the CARS Director from the region where the event where the event is to take place and the CARS president or his/her delegate, event organisers may seek limited event exemptions to both the GCRs and NRRs. Such exemptions shall be specifically noted in the event rules and regulations.

1.10 Automobiles

1.10.1 Dangerous Construction

The Steward(s) of the Meeting may exclude any automobile the construction or condition of which is deemed to be dangerous. In events where a steward is not required, the organizer or scrutineer shall have this right.

1.10.2 Suspension or Disqualification of a Particular Automobile

CARS may suspend or disqualify a particular automobile in consequence of a breach of these GCRs and/or the National Rally Regulations by the entrant.

1.10.3 Advertisements on Automobiles

Advertisements are permitted on automobiles in competition within the limits laid down in the current CARS National Rally Regulations. Name(s) of the entrant, driver(s), co-driver/navigator(s) and/or service crew may be inscribed on the automobile. These inscriptions shall not be considered advertising. No sign, label, lettering or paint job shall be so arranged as to make competition numbers or other identification illegible to officials or marshals at an event to the satisfaction of the Steward(s) of the Event.

2. TERMINOLOGY

The following descriptions are adopted for use in these GCRs, in the Rally Regulations of CARS, in the appendices thereto, in all Supplementary Regulations and for general use.

ASN

A National Automobile Club or other National Body recognised by the FIA as holder of sporting power in a country. In Canada this organisation is ASN Canada FIA.

AUTOMOBILE

A land vehicle propelled by its own means, running on at least four wheels not in a line, which must always be in contact with the ground and of which at least two must effect steering and at least two the propulsion.

CARS

The Canadian Association of Rallysport. A non-profit organisation which is the governing body for rallysport in Canada.

CARS AFFILIATED CLUB

Any motorsport club in Canada which is affiliated with CARS.

CLERK OF THE COURSE

The organizer(s) of an event may appoint a clerk of the course (and one or more assistants) to be responsible for conducting all or part of the competition in accordance with CARS applicable regulations and the event's supplementary regulations.

CLOSED EVENT

A competition confined solely to the members of the club or clubs organising the competition and invited competitors.

CLUB

Any body recognised by CARS as a club.

CLUB EVENT

A competition which is not part of a regional or national championship.

CO-DRIVER

In a performance rally, the person normally charged with interpreting the route and timing instructions. Also known as second driver.

COMPETITION

A contest in which an automobile takes part and which is of a competitive nature or is given a competitive nature by publication of results.

COMPETITOR

A person who competes in any event, whether as a driver, co-driver or navigator.

COURSE

The route to be followed by a competitor in a competition.

CREW

A crew is made up of two persons designated on the official entry form as Driver and Co-Driver/Navigator.

DISCIPLINE

All CARS competitions are categorised by type of activity or "discipline" (e.g. navigational, performance, Rallycross, rally sprint etc.).

DRIVER

A person nominated as the driver of an automobile in any competition.

DURATION OF A RALLY

Any rally starts with the registration and/or scrutineering (including, if applicable, checks on the spare parts of the car) and ends upon the expiry of one of the following time limits, whichever is the later:

- Time limit for protests or appeals or the end of any hearing.
- End of the administrative checking and post-event scrutineering carried out in accordance with national rally regulations.
- End of the prize giving.

ENTRANT

A person or organisation whose entry is accepted for any competition.

EVENT

A program of one or more competitions.

FIA

Federation Internationale de l'Automobile, the International Federation of National Automobile Clubs.

FISA

Federation Internationale du Sport Automobile, the International Automobile Sport Federation, appointed by the FIA, to deal with competition matters.

FORCE MAJEURE

A condition or conditions over which the organisers have no control, yet play a part in determining the outcome of a competition.

FULL INTERNATIONAL COMPETITION

A competition which is entered on the International Calendar and is open to competitors and drivers of various nationalities.

LEG

A leg is a major portion of a rally designated by the organizers as such in the event Supplementary Regulations and/or the route book whereby it is necessary to complete the leg within maximum lateness.

LICENCE

A certificate of registration issued by CARS or an ASN to any person wishing to take part in competitions.

NATIONAL EVENT

A competition which is open only to competitors and drivers holding an appropriate licence issued or recognised by CARS and conducted under the organising permit issued by CARS.

NATIONAL SERIES MANAGER

CARS official, appointed by the CARS Board, charged with management of the Canadian Rally Championship.

NAVIGATIONAL RALLY

An event which is entirely or partly run on roads open to normal traffic where the accent is on correctly following route and timing instructions. Performance is usually determined by early and late arrival at checkpoints along the route. These events are also known as TSD events (Time, Speed and Distance).

NAVIGATOR

In a navigational rally, the person normally charged with interpreting the route and timing instructions.

NEUTRALISATION

Time during which the crews are stopped by the rally organisers for whatever reason.

NON-SPEED EVENT

An event in which speed is not the determining factor, although timing of the vehicles is usually considered as in a driving skill test, navigational rally, concourse, gymkhana, slalom, etc.

OPEN EVENT

A competition in which competitors are comprised of members of any recognised motorsport club or the public at large.

ORGANISER(S)

A person or persons approved by CARS and authorised by promoters, clubs or other groups to organise an event on their behalf. Where an organiser is appointed to act on behalf of promoters, such organiser shall take the place of such promoters for the purposes of these GCRs and shall be deemed to be the agent of the Promoters.

ORGANISING PERMIT

A document, granted solely by CARS, authorising the organisation and holding of a competition.

PARC EXPOSÉ

A location where competitors are required to display their cars for public viewing.

PARC FERMÉ

Area in which no repairs or outside assistance is possible, except in the cases expressly provided for by the CARS national rally regulations and by the supplementary regulations of the rally.

PERFORMANCE RALLY

An event run on both public and controlled roads. Performance is mainly determined by the length of time to complete the stage portion of the event.

PROGRAM

A document prepared by the promoters and/or organisers of an event for the purpose of informing the participants and spectators about such a meeting.

PROMOTER(S)

Any person or body (other than organisers) proposing to hold or holding an event.

RALLY REGULATIONS

Set(s) of regulations, in addition to these GCRs, which shall be issued from time to time by CARS and/or CARS Regions to provide additional rules and regulations outside of these GCRs to further define navigational and performance rallies and how they are to be run.

RALLYSPRINT

A form of rallysport where competitors compete one car at a time against the clock on unpaved surfaces. Rallycross events, run on temporary courses at one venue, are a form of Rallysprint.

REGIONAL EVENT

An event which occurs within a region, is administered by the region, and is part of that region's performance or navigational championships.

REGROUPING

Stop scheduled by the organisers between 2 time controls to enable the schedule to be followed on the one hand, and on the other, to regroup the cars still in the rally. The stopping time may vary from crew to crew.

RESTRICTED COMPETITION

An event which is confined to persons having some particular qualification.

SANCTIONED EVENT

Any event or competition authorised and approved by CARS and having the appropriate organising permit if one is required by these GCRs or the National or Regional Rally Regulations.

SECTION

All the parts of a rally between:

- The start and the first regrouping halt or service control.
- Two successive regrouping halts or service controls (or combination).
- The last regrouping halt or service control and the finish of the leg or the rally.

SERVICE CREW

A person or persons registered by the entrant at an event to be able to service a vehicle entered in the competition.

SPEED EVENT

An event in which vehicles run individually (even though two or more may be in motion simultaneously) and in which the relative performance of the competitors is assessed by timing them over a given distance.

STAGE

A portion of a performance rally route, run on controlled roads, which is used as a speed test.

STEWARD

CARS official charged with representing CARS at an event by enforcing these GCRs and all other regulations governing the event.

SUPPLEMENTARY REGULATIONS

Regulations drawn up by the organisers of a competition or competitions and approved by CARS with the object of laying down details of such competitions. They are supplementary to these GCRs and the Rally Regulations of CARS.

TECHNICAL DIRECTOR

CARS official, appointed by the CARS Board, charged with technical matters relating to vehicle eligibility in CARS events as well as maintaining CARS national regulations.

TIME CARD

Card intended for the stamps and entry of times recorded at the different control points scheduled on the itinerary.

TRANSIT

A portion of a performance rally route which is run on public roads open to normal traffic.

3. COMPETITIONS IN GENERAL

3.1 Organisation of Events

CARS-sanctioned events may officially be organised by:

- (a) CARS National;
- (b) A CARS Region;
- (c) A CARS Affiliated Club;
- (d) Other Clubs or Promoters approved by CARS.

3.2 Sanctioned Event

Any event organized as per 3.1 above and having an event insurance certificate satisfactory to all sanctioning requirements is a CARS sanctioned event and is open to entrants and CARS officials (subject to other provisions of these GCRs).

3.3 Required Approval

The name or emblem of the CARS shall be associated only with events sanctioned by CARS. Organisers shall not distribute entry forms or supplementary regulations for a CARS event prior to obtaining CARS sanction.

3.4 Knowledge of and Submission to the Rules

Every person, body, group of persons, etc., organising a competition or taking part therein shall by doing so or by and upon applying for an organising permit or by and upon applying for a licence from CARS or by and upon entering a competition, be deemed to have and recognise that they have:

- (a) Made themselves acquainted with these GCRs;
- (b) Submitted themselves without reserve to the consequences resulting from these GCRs and any subsequent alteration thereof;
- (c) Renounced the right to have recourse to any arbitrator or tribunal not provided for in the GCRs;
- (d) Agreed to exonerate and keep indemnified the Promoters, CARS and its subsidiaries, and their respective agents from and against all liability whatsoever to any such person or body or group of persons respectively in respect of, or in connection with any meeting, competition or event held under these GCRs, from whatever cause arising or alleged to arise and notwithstanding that the same may have been contributed to, or occasioned by, the negligence of the said bodies, their agents, officials, servants, or representatives;
- (e) In the case of entrants for and competitors in events, they shall be deemed to have agreed in the circumstances aforesaid to exonerate and keep indemnified all and any other competitors, their servants, or agents from and against all liability whatsoever to such entrants or competitors in connection with the driving of their automobiles or any other act, omission or occurrence during the course of a competition;
- (f) Agreed as set out in sub-paragraphs (d) and (e) of this Rule with each and all the persons or bodies referred to in those sub-paragraphs and so that each and any of those persons or bodies shall be entitled to the benefit of such agreements.

3.5 Right of Observation

CARS reserves the right to appoint members of the CARS Board or their designates and/or CARS steward(s) to observe and evaluate any competition and/or any person officiating, working or participating in any competition.

3.6 Official Documents

For every competition of a championship series, the Promoters and/or Organisers are responsible for drawing up and publishing Supplementary Regulations.

3.7 Announcement to be Made on All Official Documents

All Supplementary Regulations, programs, entry forms, results and advertisements relating to any competition shall bear in a conspicuous manner the following announcement: "Held under the Rally General Competition Rules of CARS".

3.8 Authorisation of Description

No event may be described as "International", "National", "Regional", "Championship" or given any title or status which is not authorised by CARS.

3.9 Postponement, Abandonment or Cancellation of a Competition

A meeting or competition forming part of an event shall not be postponed, abandoned or cancelled unless:

- (a) Provision for so doing is made in the Supplementary Regulations,
Or;
- (b) The Stewards of the event have so ordered.

In the event of cancellation or postponement for more than twenty- four hours, entry fees shall be returned if so directed by CARS.

3.10 Advertising and Promotion of Events

All advertising or promotional material used in connection with competitions must state that the event is sanctioned by The Canadian Association of Rallysport and, if applicable, by the FIA.

Advertising material must be in good taste and must not be misleading or untruthful. The words "International", "National", "Championship", etc., when used to describe an event or used in the title of an event must be approved by CARS.

Any competitor or other person or body advertising the results of a competition, a series, or a championship must state that the competition was sanctioned by The Canadian Association of Rallysport, the exact conditions of the performance referred to, the nature of the competition, the category and the class of automobile, and the position or results obtained, or such additional information as CARS may require. All advertisements that are published prior to the issuance of Official Results by the Promoter(s) or organiser(s) must contain the words "Subject to Official Confirmation".

The publication of an advertisement relating to the results of a competition that is drawn up in such a way which could mislead the public, or the infraction of this Rule (whether by way of omission from or addition to the particulars required to be stated) shall render the person(s) or body by whose authority or on whose behalf the advertisement is published or issued liable to the penalties provided by these GCRs and may entail the infliction of a penalty on the person(s) responsible for authorising the advertisement.

Publication under this Rule shall be construed to mean any or all of the following:

- (a) The airing of commercial message by means of radio, television film or electronic formats;
- (b) The publication of printed advertisements in newspapers, magazines other periodicals or electronic formats;
- (c) The publication of brochures, booklets, forms, signs or displays.

Competitors who allow their names or photographs to be used in connection with misleading advertising shall be liable to disciplinary action from CARS.

3.11 Commercial Rights

CARS is the sole holder of the commercial rights for visual images, and filming or recording of rallies run under its sanction.

Every person, body, group of persons, etc., organizing a competition or taking part therein shall by doing so or by and upon applying for an organizing permit or by and upon applying for a licence from CARS or by and upon entering a competition, be deemed to have and recognize that they:

- (a) Recognize that commercial rights from sanctioned events belong to CARS;
- (b) Agree that CARS may release or use particulars concerning participants in any of the events including images of the participants for commercial purposes by CARS;
- (c) Agree that CARS may access and use these images or film as part of its grievance and disciplinary proceedings as well as in determining violation/compliance of its sporting regulations.

Competitors may install in-car cameras and film/record their participation in the event subject to the above regulations.

4. ORGANIZATION OF EVENTS

4.1 Necessary Permission and Approval

No competition shall be held unless CARS has signified its approval by granting an organising permit or has waived the necessity of a permit under its Regulations. CARS may attach conditions to the granting of a permit or decline to grant a permit or withdraw a permit without stating any reason.

4.2 Recognition of National and Local Authorities

A competition may be held either on the road or on the track or both, but no permit shall be granted by CARS or competition be held unless the promoters or organisers shall have first obtained such official permission as may be necessary from the competent governmental or local authority. Competitions taking place on roads open to the public must conform to the traffic laws of the jurisdiction in which they take place. Any breach of such laws by a competitor during a competition shall be a breach of these GCRs and may be dealt with accordingly.

4.3 Application for an Organising Permit

For every regional or national championship event, an application for an Organising Permit shall be made in writing. The application must state:

- (a) The name and address of the applicant;
- (b) The body or person on whose behalf the application is made and the official position held by the applicant;
- (c) The nature of the competition for which the permit is required;
- (d) The date(s) and place(s) of the proposed competition;
- (e) In the case of an application for a permit to hold a speed event on a track or circuit which is not already approved by ASN, an application for approval of the track or circuit must be made to CARS and this application must be accompanied by a plan of the track or circuit to a scale of not less than four centimeters to the kilometer. This plan shall give details of the course and indicate the proposed position of spectators, fencing and other safety arrangements.

For complete details on application procedures, see the CARS National Rally Regulations.

4.4 Permit Fees

The scale of fees payable to CARS on granting of an organising permit is laid down by CARS.

4.5 Supplementary Regulations

Competitions sanctioned by CARS must be run in compliance with the rules and provisions provided by CARS. However, organisers must provide for every National or Regional Championship competition a set of Supplementary Regulations which need not repeat and may not, under any circumstances, conflict with these GCRs and other CARS National Rally Regulations unless approval for specific exemptions has been given as per GCR 1.9. The Supplementary Regulations must contain the following information:

- (a) The name and nature of the proposed competition(s);
- (b) The name and address of the promoters and/or organisers;
- (c) An announcement that the proposed competitions(s) will be "Held under the CARS Rally General Competition Rules, the National Rally Regulations and/or the Regional Rally Regulations" as applicable;
- (d) The place, date and times of the event;
- (e) The dates of opening and closing of entries and how and where they shall be made;
- (f) The amount of the entry fee and any other fees (organizer supplied course notes);
- (g) The names of the Steward(s) of the Event and other officials if available;
- (h) All such other information and requirements as shall from time to time be specified in the relevant CARS National and/or Regional Rally Regulations.

4.6 Alteration of Supplementary Regulations

No alteration shall be made to the Supplementary Regulations after the commencement of the period for receiving entries unless:

- (a) CARS and all competitors already entered agree to the alteration, or;
- (b) The Steward(s) of the Event so decide for reasons of safety or force majeure, or;
- (c) The Steward(s) of the Event may so authorise under exceptional circumstance.

4.7 Contents of Program

If a program, spectator guide or similar document is published, it shall contain the following information:

- (a) A statement that the event is held under the Rally General Competition Rules of CARS;
- (b) The name of the Promoters and/or Organisers;
- (c) The place and date of the event;
- (d) A short description and schedule of the proposed competition;
- (e) A description of the arrangements for spectators;

4.8 Entry

An entry made and accepted in accordance with these GCRs and any relevant Supplementary Regulations is a contract between a competitor and the promoters or organisers by which the competitor agrees to take part in the competition entered unless prevented by force majeure and the organisers agree to comply with the conditions of entry provided that the competitor has made every effort to take part in the event. A breach of such contract may be treated as a breach of these GCRs.

4.9 Withdrawal

An entry may be withdrawn:

- (a) without penalty, up to 48 hours before the start of registration;
- (b) with up to 25% of the entry fee in penalty, less than 48 hours before the start of registration (percentage at the organiser's discretion);
- (c) with up to 100% of the entry fee in penalty after the opening of registration (percentage at the organiser's discretion).

The withdrawal shall be made in writing by facsimile or by telephone. An entrant or competitor accepted to take part in a competition who withdraws other than in the above manner or who does not take part in that competition but takes part in another on the same day may be held in violation of these GCRs.

4.10 Contents of Entry Form

Entry forms shall contain the following:

- (a) Space for the full names and address of entrants and competitors;
- (b) Space for the signature of the entrant and competitors;
- (c) The following statement: "If an entrant or competitor is under the legal age of majority, this form must be countersigned by the appropriate parent or guardian.";
- (d) A statement to the effect that the event is held under the Rally General Competition Rules of CARS;
- (e) Any other information required by the Supplementary Regulations to be stated on the form;
- (f) Every entry form shall contain the following statement which every entrant and competitor shall agree to by signing the entry form before being allowed to take part in any competition:

"I have read the CARS Rally General Competition Rules, the CARS Rally Regulations and all other regulations in effect for this event and agree to abide by and be bound by such rules and regulations. I further agree to ensure that the competitors nominated by me and all crew members are similarly acquainted with these rules and regulations and agree to abide by them and be bound by them".

"The entrant, in signing this entry form and agreement and in consideration of the right to enter and compete in the event, agrees to participate in the event at his/her own risk. The entrant further agrees that he/she, the competitors, and all crew members shall execute a release agreement prior to their admission to the event."

In the case of an event taking place wholly or partly on the public roads or highways, the entry form shall also include the following:

"I declare that the use of the car hereby entered is covered by insurance as required by (the appropriate highway traffic act), which is valid for such parts of this event as shall take place on roads as defined in that Act."

4.11 Closing of Entries

The date(s) and time for the closing of entries shall be specified in the Supplementary Regulations for the event.

4.12 Entry Containing a False or Incorrect Statement

An entry which contains a false or incorrect statement shall be null and void and the entrant may be deemed guilty of a breach of these GCRs. The entrant may also be subject to entry fee forfeiture.

4.13 Refusal of an Entry

In the case of an entry for a competition being refused by the organising committee, the latter must notify the applicant of such refusal stating the grounds for such a refusal. If an advance entry for any competition is refused, notification of such refusal shall be sent to the entrant by telephone, by electronic means and/or by mail at the address given on the entry form as soon as possible and at least five days before the event. In the case of late entries or entries received at the event, the entrant shall be notified of a refusal immediately upon the entry being received.

4.14 Conditional Acceptance of Entry

Supplementary Regulations may provide that entries may be accepted under certain clearly stated conditions. Conditional acceptance shall be notified to the entrant by letter, telephone or electronic means not later than the date following the closing of entries. If the number of entries exceeds the maximum number of competitors laid down in the Supplementary Regulations, those to be accepted shall be selected in the manner specified by the Supplementary Regulations. If no manner is specified, they shall be selected as the organisers shall decide.

4.15 Change of Automobile

Subject to anything to the contrary in the applicable regulations, after the publication of the program the Steward(s) of the Event may authorize the substitution in an event of an automobile different from that specified on the entry form, provided that the Supplementary Regulations are complied with in all other respects.

4.16 Instructions to Competitors

Instructions to competitors may be issued to deal with particular points arising after the issue of the Supplementary Regulations, but if and so far as such instructions are contrary to these Rally GCRs, other applicable regulations or the Supplementary regulations, they shall be null and void, unless authorized by the Steward(s) of the Event.

4.17 Insurance

- (a) CARS requires that all CARS member clubs have both General Liability and Directors' and Officers' Liability insurance coverage and that all events have an event insurance coverage satisfactory to the board's requirements. For further details, contact the CARS National office.
- (b) It is a condition of the ASN Canada FIA insurance policy that waivers in the form specified by the insurance company must be signed by all persons who participate in an event as a worker, official, competitor, service crew or who are permitted to enter areas normally closed to the public or spectators.

4.18 Publication of Results

Organisers of events shall make Official Results as detailed in NRR 20.5.

4.19 Payment of Prize Money and Presentation of Awards

The organisers shall distribute all prize money to the entrants as promptly as possible after the official results of a competition shall have been arrived at, or within such period as CARS shall allow. Any awards shall be presented within a like period unless the Supplementary Regulations specify a particular date or occasion for presentation.

5. COMPETITORS

5.1 Licencing of Competitors

Every person who makes an entry for or who competes in an event held under CARS rules shall be in possession of a current, valid competition licence of the grade required for that type of event.

The CARS Board of Directors may authorise specific events or types of events for which a competition licence is not required.

5.2 Grant of Licence

Application for and the granting of a licence shall be governed by the conditions laid down in the current CARS National Rally Regulations. The holding of a licence by an individual is a privilege and not a right. A licence issued by CARS may, for sufficient cause, be withdrawn or suspended at any time.

A CARS issued licence shall be suspended by failure to pay a CARS levied fine within the specified time period or by failure to pay outstanding fees to an event and shall remain until all payments are received.

5.3 Right to Hold a Licence

The principle that will apply in all cases is that any applicant who qualifies for a licence within these GCRs and the National Rally regulations shall be entitled to such a licence.

5.4 Period of Validity of Licence

Licences shall be valid from the date of issue to the end of the current calendar year, or such other date(s) CARS may specify.

5.5 Licence Fees

For a licence to be granted by CARS, a fee shall be payable to CARS in accordance with a scale laid down from time to time.

5.6 Validity of Licence

- (a) A competitor's licence shall be signed in ink by the holder on receipt and shall not be valid until so signed.
- (b) An International Rally Licence issued by ASN Canada FIA shall make the holder eligible to enter, compete and score championship points in all competitions organized under the permit of CARS, subject to the right of refusal by the organizer and limits imposed by region regulations for eligibility requirements in regional events.
- (c) A driver's licence granted by CARS does not constitute a certificate of competency of the holder. No driver, though holder of such a licence, who is suffering from any disability, permanent, temporary, or otherwise, which prejudicially affects the normal control of his/her facilities shall compete in any event. Provided that where a driver is suffering from a permanent disability, the nature of which he/she has declared to CARS who have nevertheless issued a licence, such disability shall not debar the driver from competing in an event in compliance with any conditions endorsed on his/her licence.
- (d) If the holder of a licence granted by CARS is, during the currency of such licence, disqualified from holding a licence to drive automobiles on the highways in any province or territory of Canada, then his/her licence granted under these GCRs shall become null and void and shall forthwith be returned to CARS. The CARS Board may, upon application by the licensee, consider the circumstances under which the disqualification was ordered and, if it thinks fit, authorise the reissue of such competitor's competition licence.
- (e) For CARS events which require a competitor to hold a competition licence, Canadian citizens and those residing in Canada must hold a licence issued by CARS or ASN Canada FIA.
- (f) Canadians may only hold licences issued by ASNs of other countries after receiving written authorisation to do so from the ASN for Canada.

5.7 Production of Licence

A competitor at an event shall produce his/her licence on demand to an official of that event.

5.8 Medical Responsibility of Driver

Any licence holder who suffers injury or illness that affects his/her medical fitness to participate in events shall report this injury or illness immediately to the CARS Medical Delegate and be recertified by him/her or another physician he/she designates before competing in further CARS events. The CARS Medical Delegate may order the suspension of competition privileges of any CARS licence holder until the licence holder has been recertified by the CARS Medical Delegate or another physician.

5.9 Assumed Name

If a licence is required in an assumed name, special application therefor shall be made to CARS which may, if it thinks fit, issue a licence in such name. A person granted a licence in an assumed name shall, so long as he/she holds a licence in that name, not take part in any competition except under the name shown on the licence.

5.10 Responsibility of Entrant, Competitors or Others

The entrant shall be responsible for all acts or omissions on the part of his/her competitors, or service crew, but each of these shall also be responsible for any infractions of these Rally GCRs, CARS Rally Regulations, Supplementary Regulations or instructions to competitors.

5.11 Alcoholic Beverages

Consumption of alcoholic beverages by any competitor, entrant, service crew member, event worker or official before the end of any day's competition activities at an event is expressly forbidden. Each entrant or competitor shall be responsible for the conduct of his/her crew. Any competitor, crew member or official who has consumed any alcoholic beverage before the end of the day's competition activities shall not participate nor shall he/she be present or remain in any area likely to cause embarrassment to any competitor, crew member, organiser or official.

5.12 Narcotics and Dangerous Drugs

The use of any narcotic or dangerous drug by any competitor, service crew member, event worker or official prior to or during a CARS event, is specifically prohibited. The Steward(s) of the Meeting may prohibit any individual suspected of being under the influence of any narcotic or dangerous drug from competition in that event and further disciplinary action may be taken by CARS.

5.13 Competitor Review

The Regional or National discipline directors are authorised to convene a court to review a competitor's conduct, car legality, competition record and/or other matters. Such a court shall have the power to invoke penalties as specified in these GCRs and may revoke licences. The driver shall have the right to appeal this court decision as specified in these GCRs.

5.14 Conduct of Entrants, Competitors and Crew

Every entrant, competitor or crew member at a CARS sanctioned event shall be bound by the CARS Code of Conduct and shall conduct himself/herself according to the highest standards of behaviour and sportsmanship, particularly in relationship with other competitors and officials, and in a manner that shall not be prejudicial to the reputation of CARS or to the automobile sport. Failure to do so may be treated as a breach of these GCRs.

6. OFFICIALS AND THEIR DUTIES

6.1 Officials

6.1.1 Event Officials

The staff of officials whose duty it is to direct and control the competition shall be:

- Organiser/Coordinator
- Clerk of the Course
- Chief Control Marshal
- Chief Scorer
- Chief Scrutineer
- Stage Captains
- Registrar
- Chief of Security (Safety Officer)
- Chief Radio Marshall (or Chief Communications Officer)

Event Officials may have assistants to whom any of their duties may be delegated.

6.1.2 CARS Officials

Those, when present at a CARS sanctioned event, who will be designated as an official and may have a supervisory role during the competition. These officials may have additional specific duties and authority at an event as outlined in these GCRs and/or the National Rally Regulations.

- Steward(s) of the Event
- President
- National Series Manager
- Technical Director
- CARS Directors (non-competing)

6.2 Stewards

6.2.1 Required Stewards

At least one CARS appointed Steward must be available for all or part of each CARS sanctioned performance rally competition. CARS Steward(s) may also be appointed for navigational events.

6.2.2 Assignment

All Stewards are CARS officials and, as such, receive assignments from and report to the CARS Board member in their region or to the region executive rally steward (if one has been appointed) who in turn reports to the CARS Board member.

6.2.3 Authority & Duties of the Stewards

The Steward(s) of the Meeting shall be responsible solely to CARS for enforcing compliance with the regulations governing the event.

The Steward(s) shall settle any claim which might arise during an event, under reserve of the right of appeal.

When CARS has appointed more than one Steward(s) of the Meeting, such Steward, or if more than one, the Steward appointed as the Senior Steward, shall act as chairman of the Stewards of the Meeting who shall act as a committee. The Senior Steward is, in particular, responsible for assigning stewarding duties, planning and holding meetings and for ensuring that the event report is prepared and submitted on time. When more than one Steward is appointed, one of the Stewards will be designated as the safety Steward.

The Stewards of the Meeting have power in accordance with these GCRs to:

- (a) Settle any protest or dispute arising during an event, subject to the right to appeal;
- (b) Inflict penalties of reprimand, fine, time or exclusion;
- (c) Prohibit from competing any competitor or any vehicle which they consider to be dangerous;
- (d) Exclude from any one competition or from the event any competitor who, or any automobile which, they consider as ineligible to take part therein, or whom they consider guilty of misbehaviour or unfair practice;
- (e) Penalise and/or order the removal from the rally and its precincts any competitor who refuses to obey the order of a CARS or Event official;
- (f) In case of force majeure or for reasons of safety either postpone, abandon, or stop a competition or stage. If a stage is interrupted, the Steward(s) may order the stage to be scored as provided for in NRRs. If a competition is stopped, the Steward(s) may declare it "no contest"

- and arrange for it to be restarted or alternatively declare the stage cancelled or the event concluded and determine the results based on the positions of the competitors at that time;
- (g) Render judgements or dispositions as required by the National Rally Regulations.
 - (h) Appoint a temporary substitute or substitutes to replace any Steward not able to perform his/her duties;
 - (i) Enquire into allegations of reckless driving and, if appropriate, penalise the individual and/or refer the matter to CARS for hearing;
 - (j) Authorise amendments to the Supplementary Regulations for reasons of safety or force majeure;
 - (k) Accept or refuse any correction proposed by a Judge of Fact;
 - (l) Authorise the change of driver or automobile;
 - (m) Authorise the modification of the position of the starting or finishing lines of a stage or the alteration of the schedule at the request of the organiser where necessary to ensure reasonable safety for competitors and spectators;
 - (n) Authorise the amendment of the results of a competition based on a correction by the Chief Scorer to take into account a penalty assessed against a competitor.

6.2.4 Steward's Report

As soon as practicable after the conclusion of an event, the Senior Steward shall compile, sign and send a Steward's report to:

- (a) The organiser;
- (b) The Region Rally Director;
- (c) The CARS National Office (regional or national performance rally events);
- (d) The CARS President (regional or national performance rally events).

The report must give the results, together with particulars of all protests lodged, action taken thereon, and penalties imposed together with recommendations in respect of such cases.

The report shall also contain the Steward's general comments on the organisation of the event and the exercise of their own powers in relation thereto and any other observations as to the conduct of the event which they consider should be made to CARS.

The report shall also contain a copy of the senior steward's instructions to the organiser and details of all accidents in which personal injury or property damage are believed to have occurred, detailing names and addresses of those involved.

6.3 Judges of Fact

6.3.1 Persons at an event who are deemed to be judges of fact:

- Event Officials as listed in 6.1.1
- CARS Officials as listed in 6.1.2
- Stage Captains
- Observation Control Marshals
- Start Line Marshals (for false start infractions)
- Other Event Marshals as identified in the event's supplementary regulations

6.3.2 Judges of fact may perform one or more of the following:

- (a) point out violation of Parc Fermé rules ~~any~~ false start, shortcutting a stage or striking a chicane to the organiser;
- (b) point out a) improper procedure infractions (NRR 17.3) or b) any violations at an Observation Control to the organiser;
- (c) point out some other fact of the same type as laid down in the Supplementary Regulations.

6.3.3 A protest may not be made against the decision of a Judge of Fact. His/her decision shall be accepted as final unless corrected as hereinafter provided.

6.3.4 A mistake by a Judge of Fact may be corrected by him/her with the concurrence of the Steward(s) of the Event.

6.3.5 At the close of an event, each Judge of Fact shall give to the Organiser, a report of his/her decisions.

6.4 National Series Manager

The National Series Manager is appointed by the CARS Board and reports directly to and works under the direction of the CARS President and CARS Board of Directors. The National Series Manager is responsible for the management of the Canadian Rally Championship in respect to the following:

- (a) Co-ordination with Marketing, Advertising and Sponsorship Director the aspects of Sponsorship, Marketing and Media promotions of the sport
- (b) Television
- (c) Media promotions
- (d) Ensuring uniform standards for the series from event to event
- (e) Co-ordination with event organizers regarding supplementary regulations and event safety plans
- (f) Co-ordination with CARS Technical Director regarding series scrutineering program
- (g) CARS regulation sets

6.5 Technical Director

The CARS Technical Director is appointed by the CARS Board and reports directly to and works under the direction of the CARS President and the CARS Board of directors. The Technical Director is responsible for the management of technical matters in terms of competitor personal safety and vehicle eligibility for CARS sanctioned rallies in respect to the following:

- (a) Research and development of vehicle eligibility, including establishing future direction
- (b) Co-ordination with event scrutineering teams to ensure uniform standards across the country
- (c) Development and implementation of a scrutineering program for the Canadian Rally Championship
- (d) Management of the rule change process
- (e) Management and publication of CARS regulations

The CARS Technical Director has the authority to make a final decision regarding eligibility of a vehicle or vehicle component or system. Any decision made by the Technical Director while at an event under the control of a Steward(s), shall be subject to the protest and appeal processes set out in these regulations for that event, but shall be final upon conclusion of the event unless successfully protested or appealed.

6.6 National Marketing Manager

The CARS Marketing Manager is appointed by the CARS Board and works in conjunction with the Series manager. The Marketing Manager reports directly to and works under the direction of the CARS President and the CARS Board of Directors. The Marketing Manager is responsible for the management of the marketing and advertising of the sport, the search for sponsorship and advertising opportunities and managing the Marketing and sponsorship committee that help to accomplish these duties and projects.

7. PENALTIES

7.1 Breach of GCRs

Any of the following offences, in addition to any other offences specifically referred to previously or hereafter, shall be deemed to be a breach of these GCRs:

- (a) Bribery or attempt to bribe anyone connected with the competition and the acceptance of, or offer to accept, a bribe;
- (b) Any action having as its objective participation in the competition of a person or automobile known to be ineligible;
- (c) Any fraudulent proceeding or act prejudicial to the interests of CARS or of automobile competition generally;
- (d) Competing for, accepting, or offering to accept, or advertising an award, in the nature of a title or championship, in respect CARS sanctioned events unless such award is recognised by CARS;
- (e) Reckless or dangerous driving;
- (f) Failure to obey directions or orders of Event or CARS officials;
- (g) Refusing to co-operate with, interfering with, or obstructing the actions of the Steward(s) of the Meeting or courts in the performance of their duties;
- (h) Abusive language or behaviour within the area under control of the organising club;
- (i) Failure to honour a cheque payable to CARS, a CARS Club or event organiser.

7.2 Penalties for Participation in Unauthorised Competition

Any person or body who shall promote, enter for, drive in, officiate at, or in any manner whatsoever take part in, or advertise, or obtain publicity for the results of, a competition not organised or held in accordance in all respects with these GCRs or who shall become disqualified or suspended by the governing body of any other discipline recognised by CARS shall be disqualified or suspended and shall there by forfeit his/her right to:

- (a) Hold a CARS competition licence;
- (b) Hold any official appointment in connection with CARS or in connection with any competition;
- (c) Be eligible for participation in competitive events;

for such time as CARS may think fit.

7.3 Penalties

Any promoter, organiser, official, competitor, entrant, or other person or organisation committing a breach of: a) the GCRs, b) the CARS code of conduct, c) regulations applicable to an event, d) conditions attached to an organising permit, e) instructions to competitors or f) event supplementary regulations may be penalised as hereinafter provided.

7.4 Imposition of Penalties

Penalties may only be assessed by the Steward(s) of the Meeting or other CARS appointed judicial body. The penalties which may be inflicted are, in order of increasing severity, as follows:

- (a) Reprimand (which may be verbal or written);
- (b) Fine;
- (c) Penalty of time;
- (d) Exclusion from a competition or event;
- (e) Loss of accrued points;
- (f) Suspension;
- (g) Disqualification;

7.5 Sentence of Reprimand or Fine

A reprimand or a fine may be imposed by CARS or by the Steward(s) of an Event. The maximum amount of a single fine is \$2,500. A fine may be inflicted on any entrant, competitor, assistant, or passenger who does not comply with the requirements of any regulation or with any instruction of the officials of the meeting.

7.6 Time Limit for Payment of Fines

Fines shall become due and payable seventy-two hours after being assessed. Any delay in making payment may entail suspension for the period during which a fine remains unpaid (See GCR 5.2).

7.7 Allocation of Proceeds or Fines

The proceeds from all fines, forfeited protest fees and appeal fees resulting from National events shall be remitted to CARS National Office. The proceeds will be used by the CARS Board for the use of promotion

and organisation of National Rally Championship Events. Where the highest status of the event is regional, proceeds shall be remitted to the particular Region Office for use by the Region Rally Director in the promotion and organisation of Regional Championship Events.

7.8 Penalty of Time

A time penalty may be imposed by the Steward(s) of the Event on any competitor who does not comply with the requirements of any regulation or with any instruction of the officials of the meeting. If the penalty is assessed during the running of the competition written notice of the penalty must be given to the entrant or competitor as soon as practical. Supplementary Regulations may specify the amount of penalty for certain infractions.

7.9 Sentence of Exclusion

A sentence of exclusion may be pronounced by the Steward(s) of an Event or by CARS and may be retroactive. Before imposing a sentence of exclusion, the Steward(s) of the Event or CARS, as the case may be, must summon the party concerned before them and afford him/her the opportunity of giving his/her own evidence. If the hearing is to be held at an event, officials must deliver written notice personally to the party concerned or the entrant. If the hearing is to be held at a later date, the summons may be delivered personally or by mail and must give reasonable notice and a reasonable opportunity for the party concerned to attend. A person, body or automobile shall be liable to sentence of exclusion:

- (a) If shown to have been forbidden by the proper authority to take part in the event, or;
- (b) Having taken any part in any competition, if shown to have been ineligible to do so, to have been eliminated therefrom, or to have been forbidden by the proper authority to participate in the event;
- (c) If considered by the Steward(s) of the Event or the officials to be guilty of misbehaviour, unfair practice or a violation of regulations.

Any entry fee paid by or in respect of the person, body or automobile sentenced shall be forfeited to the organisers or promoters.

7.10 Loss of Accrued Points

Loss of accrued points may be imposed by CARS. Such loss may be recommended by the Steward(s) of the Meeting or other CARS court.

7.11 Sentence of Suspension

- (a) A sentence of National Suspension may only be pronounced by CARS and shall be reserved for grave offences.
- (b) A person, body, automobile or make of automobile shall be subject to suspension when, for a certain period, forbidden by the proper authority to take part in any competition within the territory of CARS, in the case of a sentence of National Suspension, or within any country represented on the FIA, in the case of International Suspension.
- (c) Suspension shall render void any entry made for a competition taking place during such suspension and any entry fee paid or payable shall be forfeited to the organisers or promoters.
- (d) Where a sentence of National Suspension relates to a competitor or driver, he/she shall immediately send his/her licence to CARS. The licence shall be retained by CARS until the suspension expires.
- (e) Delay in handing in a licence in accordance with paragraph (d) of this rule shall automatically result in the extension of the suspension by a period equal to the delay.
- (f) Before imposing a sentence of suspension, the party concerned must be summoned either personally, in writing, or by mail giving reasonable notice and reasonable opportunity to attend a hearing at which he/she will be afforded the opportunity of presenting his/her own evidence or calling witnesses in defence or mitigation.

7.12 Sentence of Disqualification

- (a) A sentence of National Disqualification may be pronounced by CARS and shall be reserved for exceptionally grave offences.
- (b) A person, body, automobile, or make of automobile shall be subject to disqualification when expressly forbidden by the proper authority to take part in any CARS competition whatsoever.
- (c) Where the sentence of disqualification relates to a competitor or driver, he/she shall immediately return his/her licence to CARS.
- (d) Disqualification shall render void any previous entry made for any competition and any entry fee paid or payable shall be forfeited to the promoters or organisers.
- (e) Before imposing a sentence of disqualification, the party concerned must be summoned to a hearing either personally, in writing, or by mail giving reasonable notice and reasonable opportunity to attend a hearing at which he/she will be afforded the opportunity of presenting his/her own evidence or calling witnesses in defence or mitigation.

7.13 Statement of Reasons for Suspension or Disqualification

In notifying sentences of suspension or disqualification to ASN Canada FIA or other sanctioning bodies, reasons for inflicting such a penalty may be given.

7.14 Suspension or Disqualification of Automobiles

A sentence of suspension or disqualification may be pronounced on either a particular automobile or on a make of automobile.

7.15 Loss of Award

Any competitor who may be excluded, is suspended or disqualified in any competition shall thereby forfeit all rights to awards in that competition.

7.16 Amendment of Placing's and Awards

In cases of exclusion, suspension or disqualification, the Steward(s) of the Meeting or the body imposing the penalty shall declare the resulting amendment to the placing's and awards and they shall decide whether the next competitor in order (after those placed) shall be advanced.

7.17 Publication of Penalty

- (a) CARS and ASN Canada FIA shall have the right to publish or cause to be published a notice stating that it has penalised any person, body, automobile or make of automobile and, if it so desires, the reasons therefore.
- (b) The person(s) or body referred to in such notice shall have no right of action against CARS, ASN Canada FIA, another sanctioning body or against any person publishing or printing the notice and may incur disqualification if such action is taken.

7.18 Remission of Sentence

CARS shall have the right to remit the unexpired term of a sentence of National Suspension or Disqualification inflicted under these GCRs on such conditions (if any) as it may think fit.

8. INQUIRIES

8.1 Submitted to Steward of the Event

A steward of the event or an official appointed for the purpose shall be available at the start and finish of the event to receive inquiries. He/she shall also be available at the end of any leg of the rally to receive inquiries. Inquiries shall be processed according to the appropriate regulations for the event.

8.2 Time Limits for Submission of Inquiries

Time limits for submission of inquiries are:

- (a) Any inquiry by a competitor concerning eligibility of other entrants or their vehicles must be submitted within 30 minutes of the close of registration or scrutineering, whichever is later;
- (b) Any inquiry by a competitor concerning the accuracy of a marshal's watch must be submitted within 30 minutes of the competitor's time in at the finish of the rally;
- (c) Any inquiry from a competitor concerning a mistake or irregularity occurring during the rally or which only becomes evident during the rally, must be submitted within 30 minutes of the competitor's time in at the finish of the rally;
- (d) Any inquiry from a competitor concerning calculations of a score must be submitted within 30 minutes of the time the score was posted or modified;
- (e) The senior steward may grant an extension to a competitor's allowable time to submit an inquiry in order to make it physically possible for the competitor to submit the inquiry.

8.3 Types of Inquiries

- (a) Written Inquiries
 - (1) All written inquiries shall be submitted on grievance forms obtained from the steward or official appointed for that purpose.
 - (2) All written inquiries must refer to the pertinent section of the appropriate regulations.
- (b) Verbal Inquiries
 - (1) Verbal inquiries may only be made with the concurrence of the steward or stewards of the meeting.
 - (2) The steward shall keep a written record of the substance of any verbal inquiry. This record shall be signed by the competitor.

8.4 Replies to Inquiries

- (a) The organiser's reply to an inquiry must be submitted to the steward before the due time of arrival of the first car at the finish of the rally or within 30 minutes of the time the inquiry was posted, whichever is later and must contain the reason(s) for their decision(s).
- (b) The organiser's reply to an inquiry shall state specifically what changes are to be made to the scores as a result of the inquiry. The organiser shall obtain one copy of the reply and give it to the scorer.
- (c) The senior steward may grant an extension to the organiser's allowable time to reply to an inquiry in order to make it physically possible for the organiser to prepare the reply.
- (d) If the senior steward believes that sufficient extension has been granted to the organiser's allowable time limit to reply to an inquiry or if the inquirer so requests, the senior steward shall retrieve the inquiry from the organiser, mark it "void", mark the time on it and post it. (It should be noted that in so doing, the steward voids the competitor's opportunity to have the inquiry answered by the organiser.)

9. PROTESTS

9.1 Right to Protest

- (a) The right to protest lies with any competitor or driver who may consider himself/herself aggrieved by any decision, act or omission of a promoter, organiser, official, competitor, driver or other person connected with any competition in which he/she is or has been taking part, except that there shall be no right to protest against refusal of entry.
- (b) Nothing in this Rule shall affect or prejudice the right and duty of any official acting in his/her official capacity to take such action as he/she may deem proper in any circumstances, regardless of whether a protest has been lodged.
- (c) Protests against decisions of Judges of Fact in the exercise of their duties will not be admitted.

9.2 Submission of a Protest

Every protest shall be in writing and signed by the competitor making the protest. It must be accompanied by the stipulated protest fee. Unless otherwise decided by the Steward(s) of the Meeting for special reasons, the protest fee shall normally be forfeit if the protest is not upheld.

- (a) If the reply to an inquiry is unsatisfactory to a competitor or if the inquiry is marked "void", the competitor may submit a protest concerning the same matter as the inquiry within 30 minutes after his/her time in at the finish of the rally or within 30 minutes after the time the reply was posted or the inquiry marked "void", whichever is later.
- (b) If the scores are not posted at the finish, a competitor who believes that an error has been made in the calculation of a score may submit a protest concerning the error by mail. A full statement of the protest shall be sent by first class mail to the regional rally director (accompanied by the appropriate fee) with a copy to the organizer so that it will be received by the regional rally director in time for the protest meeting as listed in the provisional results.
- (c) If the Steward's decision on a protest considered at the finish of the rally is deferred, a competitor who believes that an error has been made in the revision of a score as a result of the deferred decision may enter a protest concerning the error by mail as described in section (b) above.
- (d) If the decision of an appeal board modifies scores, a competitor who believes that an error has been made in the revision of a score as a result of that decision, may enter a protest concerning the error by mail as described in section (b) above.

9.3 Adjudication of Protests

Any protest arising out of an event shall be adjudicated by the Steward(s) subject to the rights of appeal provided in these GCRs.

9.4 Hearing of Protests

The hearing of a protest shall take place as soon as practical after lodging of the protest. All parties concerned shall be given adequate notice by the Steward(s) of any protest. They shall be entitled to call witnesses, but shall state their case personally (in the case of a company, by an officer of that company) unless prior consent has been granted by the Steward(s) to representation by another person. If the concerned persons or their representatives fail to attend the hearing, judgement may go by default. Before giving a judgement in default of appearance, the Steward(s) must be satisfied himself that the party concerned is aware of the time, date and place of hearing or has been summoned to appear. In the case of an equality of votes in a panel of Stewards, the Senior Steward shall cast the deciding vote. If judgement cannot be given immediately after hearing of the parties, they must be advised of the time and place at which the decision will be given. Judgement of the Steward(s) shall be written and copies made available to interested parties and to CARS. The judgement shall state concisely the decision of the Steward(s) and the reasons therefor.

9.5 Protest Meetings

- (a) If any protest is submitted as specified in section 9.2(a) of these GCRs, a protest meeting must be held at the finish of the rally.
- (b) Upon receiving the protest, the Steward(s) shall verify that the protest has been properly submitted and then post it. Within 30 minutes of the posting of the protest, a notice detailing the time and location of the protest meeting shall be posted.
- (c) The protest meeting shall not be held sooner than 30 minutes after the notice of protest meeting is posted, except by agreement of all parties.
- (d) During the protest meeting, all persons wishing to present evidence shall be allowed to do so.
- (e) Following the protest meeting, the Steward(s) shall prepare a reply and post it.
 - (1) If the Steward(s) has reached a decision, the reply shall contain the decision and shall state specifically what changes are to be made to the scores as a result of the decision. The organiser shall obtain a copy of the decision and give it to the scorers;

- (2) If the Steward(s) wishes to obtain further evidence (e.g.: on the route), the reply shall state that the decision has been deferred.
- (f) Any decision deferred as above must be presented or sent by first class mail to the organiser within 48 hours after the finish of the rally for inclusion in the provisional results and shall state specifically what changes are to be made to the scores as a result of the decision.
- (g) If the scores were not posted according to section 20.1 of the National Rally Regulations or if a protest decision was deferred or if the decision of an appeal board has modified the scores, the date of a protest meeting to hear protests submitted by mail shall be included in the provisional results. The meeting shall be held at least 15 days after the mailing of the provisional results and shall be closed (i.e. only the stewards shall attend).
- (h) At the protest meeting, the Steward(s) shall first verify that each protest received was properly submitted with the fee and then rule on it. The decision of the Steward(s) shall be sent by first class mail or presented to the organiser within 24 hours after the meeting and shall state specifically what changes are to be made to the scores as a result of the decisions. The organiser shall then issue another set of provisional results.

9.6 Distribution of Prizes

- (a) The distribution of prizes shall not commence until the results are final or unless the award winners will not be affected by any pending protests or appeals.
- (b) Where a protest is lodged, the distribution of a prize must, if the entitlement to the prize may be affected by the decision of the stewards, be withheld until the protest has been adjudicated and either the results of any possible appeal arising out of such adjudication are known or the time limit for notice of appeal has expired without notice of appeal having been given. The list of awards insofar as it relates to such a prize must be declared to be provisional.
- (c) If after the distribution of prizes a decision is made pursuant to these GCRs which affects the results of a competition, any competitor to whom a prize has been awarded but who is adjudged to be ineligible therefore shall return such prize to the Promoters or Organisers on demand.

9.7 Re-run

Neither the Steward(s) nor CARS shall have the power to order a competition to be re-run.

9.8 Judgement

All parties concerned shall be bound by the decision given subject only to appeal as provided in these GCRs.

9.9 Ill-founded and Vexatious Protests

- (a) If a protest is adjudged not to be well-founded, the protest fee will be forfeited.
- (b) If it is proved to the satisfaction of the Steward(s) that the author of the protest has acted in bad faith, he/she may be further penalised.

9.10 Steward(s)

- (a) The term Steward(s) used in connection with protests shall mean suitably qualified person who has been named to such position by CARS.
- (b) It is not interpreted by these GCRs that the Steward(s) must be present for the whole of the event, but he/she must be present at the time and place (or places) designated for the hearing of protests. A Steward(s) who sits in judgement of protests must not have acted in any official capacity at the event, where executive power (other than as a Steward) was a part of his/her office.
- (c) The number of Stewards that shall judge a protest is as follows:
- Three Stewards for a National or International event.
 - One or three Stewards for a Regional or lesser event.

The above requirements are notwithstanding that a greater or lesser number of Stewards may have been present for all or part of an event.

9.11 Protest Fees

- (a) The protest fee shall be:
- | | |
|-------------------------------|---------|
| • International rallies | \$1000. |
| • National rallies | \$250. |
| • Regional and lesser rallies | \$100. |
- (b) The protest fee must be held by the Steward(s) until the time limit for appeal has expired.
- (c) Costs may be assessed by the Steward(s) against the protestor, the protestee and/or the organising club.

10. APPEALS

10.1 Right of Appeal

- (a) Any person (or body) shall have a right of appeal against a sentence or other decision pronounced on them by the Steward(s). They must however, under pain of forfeiture of their right to appeal, notify the Steward(s) in writing within 30 minutes of the announcement of their decision to appeal. (For the time limits within which appeals must be made and the method by which to appeal see GCRs 10.3 and 10.5)
- (b) An appeal from a decision of the Steward(s) shall be heard and judgement shall be given thereon by a CARS Appeal Board.

10.2 Jurisdiction of the Appeal Board

- (a) No members of an appeal board may sit on a hearing who may have taken part as competitors, officials, Organisers, promoters, or sponsors in the competition concerning which a decision is to be given, or may have already participated in a decision on the affair in question or who may have been directly or indirectly concerned in the matter under consideration.
- (b) Except as provided under Rule 10.2(c) of this section, a Regional Appeal Board shall constitute a final court of appeal empowered to settle finally any dispute arising out of or in connection with the control of automobile competitions within that Region that are conducted under a Regional, Club, or lesser permit.
- (c) The decision of a Regional Appeal Board may only be appealed to a National Appeal Board with the sole consent of the CARS Board. The determination as to whether or not a National Appeal Board is warranted shall be made by the CARS Board and such decision shall be final and not subject to protest or appeal. No reason need be given for such decision.
- (d) Except as provided for in the International Sporting Code, a National Appeal Board shall constitute a final court of appeal empowered to settle finally any dispute arising out of or in connection with the control of automobile sport within Canada. Appeals arising out of a Regional, Club, or lesser event will not be heard by a National Appeal Board without their first being heard by a Regional Appeal Board. Appeals arising out of National and International events will be heard by a National Appeal Board.

10.3 Time Limits for Notices of Appeal

- (a) Where an appeal arises from a decision of the Steward(s), notice of intention to appeal must be given, in writing, to the Steward(s) along with the appropriate deposit payable to CARS within thirty minutes of the announcement of their decision. The appeal proper must be lodged before the expiration of the second day after that on which the decision appealed against was given.
- (b) When an appeal is made to CARS from any other judicial proceeding, the appeal proper must be lodged before the expiration of the second day after that on which the decision appealed against was given.
- (c) Such appeals may be lodged by facsimile transfer ("FAX"), provided a written appeal complying with these rules is mailed on the same day as that on which the "FAX" is dispatched.

10.4 Effect of Giving Notice of Appeal

Notice of appeal shall not affect the validity or operation of any decision, penalty or sentence appealed against. However, the Steward(s) of the Event, if notified of intention to appeal his/her decision, may at his/her discretion permit the competitor to continue in a competition if the matter arises during the course of an event and his/her decision on this point is not appealable. In any event the Steward(s) shall order awards which may be affected by the outcome of the appeal to be withheld pending the decision of the court of appeal.

10.5 Form of Appeal

- (a) All appeals shall be in writing, specifying briefly the decision appealed against and the grounds of appeal, and shall be signed by the applicant or his/her authorised representative. The appeal shall state the address to which communications may be sent and shall be accompanied by the appropriate deposit except where the appeal fee has to be paid to the Steward(s) in accordance with Rule 10.3(a) of this section.
- (b) Appeals arising out of National or International events or Regional Appeal Boards shall be sent to the CARS National Office. Appeals arising out of Regional, Club or lesser events shall be directed to the CARS Director of the host region or other such address as designated in the appropriate regional regulation booklet.

10.6 Hearing of Appeal

Appeal boards shall be made up of three persons with one of them serving as the chair. All three shall have the right of vote.

All parties concerned shall be given adequate notice of the hearing of any appeal and they shall be entitled to call witnesses. The hearing may proceed to judgement in default of appearance by any party or witness.

The parties concerned shall state their cases personally (in the case of a company, by an officer of that company) unless consent has been given to representation by advocates. The hearing of an appeal must take place within thirty days of the notice of intent to appeal (or appeal itself, if no intent to appeal exists) unless postponed by the Board because of unusual circumstance.

The appeal may be held via a conference call or in person dependent upon the distances involved by the parties or for other reasons. The decision as to the method to be used rests with the CARS region or CARS board dependent on whether it is a regional or national appeal board and such decision shall be final and not subject to protest or appeal. No reason need be given for such decision.

10.7 Judgement of Appeal

The Appeal Board may decide that the penalty or other decision appealed against may be waived, mitigated, increased or a fresh penalty imposed, but they shall not order any competition to be re-run. Judgements shall be written and made available to all the interested parties and CARS. A concise statement as to the decision and the reasons therefore must be stated.

10.8 Order as to Appeal Fees and Costs

When giving judgement on appeal, the Appeal Board shall make such order as to the return or forfeiture of deposits and as to costs as they may think fit.

10.9 Publication of Judgement

CARS shall have the right to publish or cause to be published a judgement on appeal and to state the names of all the parties interested. The persons or bodies referred to in such a notice shall have no right of action against CARS or against any person printing or publishing the said notice.

10.10 Appeal fees

- (a) The appeal fee shall be:

(1) International rallies	\$2000.
(2) National rallies	\$500.
(3) Regional and lesser rallies	\$200.
- (b) The appeal fee must be sent to CARS to be held until after the results of the appeal.
- (c) Costs may be assessed by the appeal board.

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The CARS Board reserves the right, upon written application, to grant specific, limited exemptions to these regulations where it can be shown that the exemption is in the best interests of the sport or in the interest of safe conduct of the sport. Such exemption requests shall be communicated to the petitioner in writing by the CARS President.

11. ELIGIBILITY OF CREW

11.1 Crew

11.1.1 A Rally crew shall consist of those two persons nominated on the official entry form as driver and co-driver.

11.1.2 Passengers will not be allowed on rallies.

11.1.3 A novice driver is defined as one who starts a rally season having started four or fewer national performance rallies as a first driver and has not finished first or second in a previous national novice championship.

11.1.4 Change of crew

- (a) During a national performance rally, no change of crew and/or vehicle as listed on the official entry form for the event is permitted and, further, the crew member listed on the entry form as driver shall operate the vehicle at all times while on special stages. Switching of duties during stages shall result in exclusion from the event, and the entry shall be recorded on the results as a DNF. Switching of duties during transit sections is permitted. See NRR 15.14.
- (b) Regions may allow a change of Co-Driver during Rally Cross or Rally Sprint events, subject to the approval of Organizer and/or Steward of the event provided proper documentation is in place. Scoring and eligibility for awards shall be as per region regulations.
- (c) Regions may allow switching of duties during Rally Cross or Rally Sprint events, subject to the approval of Organizer and/or Steward of the event and that proper documentation is in place. Scoring and eligibility for awards shall be as per region regulations.

11.1.5 A private entry is defined as one not receiving assistance from a manufacturer or distributor of the entered vehicle, outside that generally available to all competitors driving that make. A factory or works entry is defined as one receiving assistance from the manufacturer or distributor of the entered vehicle other than that generally available to all competitors.

11.1.6 Only helmets meeting one of the following standards will be accepted for competition in any performance rally and must be worn by all competitors when travelling in special stages:

- (a) FIA Homologation -FIA 8860-2004
- (b) Snell Foundation -SA 2005
-SAH 2010
-SA 2010
- (c) British Standards Institution -BS 6658-85 type A/FR including all amendments
(Must be within 10 years of the date of manufacture and the original manufacturer's production date sticker must be permanently affixed to the helmet. Any helmets certified to BS6658-85 type A/FR that do not have a production date sticker will be considered as expired~~expire on December 31st, 2013~~).
- (d) SFI Foundation Inc. -SFI spec 31.1A and SFI spec 31.2A
-SFI spec 31.1/2005

It is strongly recommended that helmets be discarded after five years, due to deterioration, or after a serious accident, which could weaken the helmet.

The competitor's name must be marked clearly on the helmet.

No helmet may be modified from its specification as manufactured, except in compliance with instructions approved by the manufacturer. Hardware may be mounted on a helmet for FIA approved HANS devices, provided the hardware is installed in accordance with recommendations provided by the manufacturer of the helmet and/or the devices. Competitors assume the risk of any modifications to their helmets. Any other modification will render the helmet unacceptable.

The fitting of earplugs and microphones may be done only in respect of the paragraph above.

Paint can react with helmet shell material and affect its protective capacity, therefore, where a manufacturer provides guidelines or restrictions on the painting or decoration of helmets; these must be followed, using only paint specified by them (air drying acrylic, polyurethane enamel or others) and

preferably a painter having their approval. This is particularly important for injection-moulded shells which are not usually suitable for painting. The shell being painted should be efficiently masked as paint penetrating the interior can affect the performance of the helmet liner. Paints requiring heat curing should not be used and any process should not exceed the maximum temperature of conditioning of the helmet in the standard to which it is approved. The manufacturer's instructions should also be consulted for any considerations on the use of stickers and transfers.

- 11.1.7 Each competitor must wear a Frontal Head Restraint system (FHR) which meets the following FIA standards:

(a) HANS® system:

HANS devices shall be approved according to FIA standards 8858-2002 or 8858-2010. Consult the FIA Technical List n° 29 to see which HANS devices are approved by the FIA.

(b) Hybrid® system:

Hybrid devices shall be approved according to FIA standard 8858-2010. Consult the FIA Technical List n° 29 to see which Hybrid devices are approved by the FIA.

It is also required to use homologated tethers which are identified by an FIA 8858 label sewn on them.

The Frontal Head Restraint system should be considered as an ensemble which involves the seat, the harnesses, the frontal head restraint unit, its tethers and helmet.

For more details, "*Guide for the use of HANS in International Motor Sport*" published by the FIA Institute for Motor Sport Safety, can be found on www.fia.com under the heading FIA Sport – Regulations – Drivers' Equipment.

- 11.1.8 All competitors shall wear at all times during the event, a one- or two-piece driving suit conforming to:

- (a) FIA homologation 8856-2000
- (b) FIA 1986 standards
- (c) SFI 3-2A/5
- (d) SFI 3-2A/1 standard with approved fire resistant underwear is acceptable.

FIA or SFI gloves and shoes are recommended. No other garments worn over driving suits are acceptable on special stages.

The suit and applicable undergarments shall be presented at technical inspection in a clean and presentable condition. Driving suits must effectively cover the body from the neck to the ankles and wrists and be in good condition, free of defects, holes, cracks, frays, etc. One piece suits are highly recommended.

Users should ensure that garments are not too tight, as this reduces the level of protection.

Embroidery sewn directly onto the overalls shall be stitched onto the outermost layer only, for better heat insulation. Backing material of badges and thread used for affixing them to the overalls must be flameproof (see Appendix 1 of the FIA 8856-2000 Standard for detailed requirements and instructions for use).

- 11.1.9 A minimum of a St. John Ambulance or equivalent "Emergency First Aid" certificate is required for all CARS licenced competitors and/or those holding FIA licences issued by ASN Canada FIA, with the following exceptions listed in 11.1.9.1 and 11.1.9.2 below. The Emergency First Aid standard is a one day (8 hr) course and provincially recognized certificates which meet the Canadian Labour Code are issued upon successful completion of the course. (Certificates from internet courses are not valid.)

- 11.1.9.1 Individuals whose first aid certification has expired are allowed to compete for 30 days from the expiry date of their first aid.

- 11.1.9.2 Media Licence holders are allowed to compete without a valid first aid certificate.

In the case of 11.1.9.1, a fine of \$25.00 per event for each uncertified competitor will be assessed and remitted, by the organiser, to the appropriate CARS region. (See NRR 11.4.1)

- 11.1.10 Competitors licenced by foreign ASNs, Rally America (RA) or National Auto Sport Association Rally Sport (NASA) licenced competitors may compete in CARS sanctioned events using their licences and their sanctioning body's competitor eligibility regulations subject to parts 11.1.10.1 and 11.1.10.2 below:

- 11.1.10.1 Each such licenced competitor wishing to compete in a CARS sanctioned event must first register with CARS and ~~pay to CARS a fee, purchase a CARS "Permit to Compete"~~ (\$50 ~~for a regional event and 75\$ for a national event~~ (Cdn.)). CARS will issue an event specific "Permit to Compete" (There is no limit to the number of events a competitor may apply for in any calendar year.). In addition, the permit will provide access for these competitors to the event insurance package provided through CARS ([All](#)

~~CARS permit holders RA/NASA competitors~~ should have personal medical coverage in place for themselves and all crew members as primary medical coverage is not part of the general liability/AD&D insurance package CARS has in place.).

- 11.1.10.2 Such licensed competitors will be eligible to score points in the North American Rally Cup (NARC), ~~and in the Canadian Rally Championship or CARS regional rally championships provided their licences are of a national or higher level~~ (Regional championship events may have other eligibility requirements that would restrict the ability to score regional championship points). Such licensed competitors are eligible to follow the process to obtain a CARS regional or national licence (11.4). For CARS regional championships, besides being a member of a CARS-affiliated club and obtaining a minimum regional licence, additional requirements may be imposed by the particular region championship rules. All CARS licence holders must conform to CARS competitor and vehicle eligibility regulations.

11.2 Documentation

- 11.2.1 For each crew, a signed, self-declaration will be submitted upon registration that declares that the crew has the following:

- (a) Provincial, or equivalent, driver's licence for any crew member listed as a driver;
- (b) Vehicle registration for the competition vehicle and any service vehicle;
- (c) Third person liability insurance covering such vehicles.

The registrar may choose to verify compliance with any aspect of the self-declaration.

- 11.2.2 The following documents must be shown upon registration:

- (a) Written permission from a parent or guardian for participation in the rally for persons under the legal age of majority for the province(s) in which the event takes place.
- (b) First aid certificate for competitors licensed by CARS or ASN Canada FIA if a first aid expiry date is not shown on the licence.
- (c) For regional championship or series stage rallies, for both crew members, a CARS regional rally licence or, for those entering under 11.1.10, ~~their regional level or higher~~ their competition licence and meet the minimum CARS competition history requirements (see 11.4.1.1(b)) and an event specific CARS "Permit to Compete".
- (d) For Canadian rally championship events, for both crew members, a minimum of a CARS national rally licence, an ASN Canada FIA international rally licence or, for those entering under 11.1.10, ~~their national level or higher~~ their competition licence and meet the minimum CARS competition history requirements (see 11.4.1.1(c)) and an event specific CARS "Permit to Compete".
- (e) For performance rallies listed on the international calendar, an international rally licence for each crew member and an international entrant's licence for the entrant.

11.3 Discipline

- 11.3.1 The organising club has the right to refuse any entry. See GCR 4.13.
- 11.3.2 An entry failing to comply with these regulations and/or with the supplementary regulations before the close of registration and technical inspection may be excluded by the organiser as the result of an inquiry submitted according to GCR 8.2(a).
- 11.3.3 A competitor may be penalised by the Steward(s) as described in the GCRs for being charged with an infraction of local or provincial traffic regulations and/or the criminal code occurring during an event, or for conduct which is unsportsmanlike or detrimental to the sport. (See the GCRs for conditions under which CARS may reprimand, fine, exclude, suspend, or disqualify persons or vehicles.)
- 11.3.4 At least one crew member of the first overall and first in each vehicle class must be present for the scheduled awards presentation. Failure to do so will result in forfeiture of all awards and trophies won by that crew.

11.4 Competition Licences

- 11.4.1 Types, conditions and grades of rally competition licences.

All licence applicants must be a member of a CARS affiliated club and be a minimum of 16 years of age. For a licence other than Media or Basic to be issued, applicants must have a valid first aid certificate per NRR.11.1.9 and must supply a completed CARS medical form.

- 11.4.1.1 DRIVER-RALLY

- (a) Basic

Valid for various specific events below regional championship performance status. These licences may be issued by a CARS region and shall be valid for the rally activity designated (ie: rallysprint/Rallycross, navigational, etc.) Costs and terms of use (beyond those listed above) shall be the responsibility of the CARS region.

(b) Regional:

Valid for any level event below national status within Canada. To be eligible to obtain a Regional licence, the applicant must first have participated in two (2) events, in any combination, from the list below:

- Rally Sprint
- Rally Cross
- Rally School
- TDS Rally

A regional licence may also be issued at the discretion of the Region Rally Director.

(c) National:

Valid for all rallies held in Canada, except those that are internationally listed. In order to upgrade from a regional to a national licence, the applicant must have entered and satisfactorily competed in a minimum of two regional performance rallies.

Attendance at a CARS/ RA/NASA Rally School may count for one of the required rallies. A national licence may also be issued at the discretion of the Region Rally Director (particularly in the case of the issuance of a co-driver licence).

(d) International:

International Rally and International Rally Historic. These licences are issued by ASN Canada FIA and when accompanied by a letter of authorisation, are valid for internationally listed rallies.

11.4.1.2 ENTRANT

An international entrant's licence (a separate licence from a driver's licence) is required in order to enter an event listed on the FISA international calendar. This entrant's licence is available from ASN Canada FIA.

11.4.1.3 SINGLE EVENT LICENCE

Upon receipt of a normal licence application, a one-time Rally Licence may be issued to a competitor subject to the following rules:

- (a) Valid only for the specific Regional or National event shown on the application.
- (b) Upgrading privileges will be allowed for first time licence holders only. If the first time licence holder decides to compete in another event in the same calendar year, he/she must reapply for a Regional/National licence and pay the difference in fees. However, no upgrading privileges will be allowed for competitors who have held a rally licence, including a single event licence, in a previous year. He/She must reapply for a Regional/National Rally licence and pay the entire cost of a replacement licence.

11.4.1.4 CO-DRIVER ONLY

CARS, or a region rally director may issue a Co-Driver only licence. The following rules apply:

- (a) The licence is for co-driving only.
- (b) It may be upgraded as per 11.4.1.3.

11.4.1.5 MEDIA LICENCE

The CARS Director from the host region may issue a Media Licence, valid for a specific event, for the purpose of gaining media exposure for Rallying. The following rules apply:

- (a) The licence is for co-driving only unless the applicant meets the requirements set out in 11.4.1.1(b) and (c). ~~(Under exceptional circumstances it may be given to the driver.)~~
- (b) The applicant must be a bona fide media representative or have recognized PR/Media value.
- (c) The applicant must be a member of a CARS club.
- (d) Only one Media Licence will be issued to any one individual in one calendar year.
- (e) No championship points shall be awarded to either member of the crew or the marque.
- (f) Holders of Media Licences are exempt from the first aid certification, but must submit a completed medical form and medical form requirements.

- 11.4.2 Competition licences must be applied for well in advance of the date for which they are required. Competition licences applied for within one week of an event for which they are required will be subject to a \$25.00 surcharge.

11.5 Entries

- 11.5.1 Entries to events must be received by the registrar of each event by the date set by the organiser.
- 11.5.2 In addition, all entries by seeds FIA 1, 2 and 3 should include a "press" package conforming to designated outlines. (The press package is also recommended for all the other seeds.)
- 11.5.3 Late entries will be accepted only if:
- (a) The limit for number of entries has not been reached, and
 - (b) A late penalty is enclosed.
- 11.5.4 Competitors may withdraw their entries:
- (a) Without penalty, up to 48 hours before the start of registration.
 - (b) With up to 25% of the entry fee in penalty, less than 48 hours before the start of registration (percentage at the organiser's discretion).
 - (c) With up to 100% of the entry fee in penalty after the opening of registration (percentage at the organiser's discretion). Also see GCRs 4.9

12. ELIGIBILITY OF VEHICLES AND EQUIPMENT

12.1 Application

12.1.1 These regulations shall apply to vehicles competing in rallies which contain special stages.

12.1.2 Vehicles must comply with these regulations at all times during the competition.

12.2 Definitions

12.2.1 Chassis - Bodywork.

- (a) Interior bodywork: cockpit and trunk.
- (b) Exterior bodywork: All the entirely suspended parts of the car licked by the airstream.
- (c) Chassis: The overall structure of the car around which are assembled the mechanical components and the bodywork including any structural part of the said structure.

12.2.2 Model.

A model is a basic manufacturer's designation (e.g.: Subaru Impreza, Volkswagen Golf, Ford Focus, Mitsubishi Lancer, etc...)

12.2.3 Model variant.

A model may exist in several variants as to bodywork (i.e.: 2 door sedan, 4 door sedan, coupe, station wagon etc.) or with regard to mechanical components (e.g.: WRX, WRX Sti, etc.).

12.2.4 Original equipment.

Original equipment is all items of standard or optional equipment that could have been ordered with any particular bodywork variant of the model, installed on the factory production line, and delivered through a dealer in Canada. This does not include special orders, "one-offs" or pre-production vehicles. Dealer installed options, except as required by manufacturer directives (no matter how common), are not included in this definition.

12.3 Safety Regulations

12.3.1 Road worthiness.

12.3.1.1 All competing vehicles must be roadworthy and, the following items in particular must be adequate and functioning properly:

- (a) All brakes
- (b) Horn
- (c) Windshield wipers
- (d) All legally required exterior lights
- (e) Tires, including all spares
- (f) Exhaust system

12.3.1.2 For each crew, a signed, self-declaration will be submitted at registration that the competition vehicle is road worthy and the above items are operating properly.

12.3.2 Roll Over Protection.

12.3.2.1 Rollcages are mandatory for all vehicles.

12.3.2.2 Specific roll over protection is subject to the approval of the scrutineer at each event.

12.3.2.3 Basic design considerations.

The basic purpose of the roll over protection is to prevent serious bodysell deformation, and so reduce the risk of injury to occupants, in the case of a collision or of a car turning over. The essential features of safety cages are sound construction designed to suit the particular vehicle, adequate mountings and a close fit to the bodysell. The safety cage must not unduly impede the entry or exit of the driver and co-driver.

All new vehicles with log-books issued after January 1, 2009 must be fitted with a safety cage built to FIA Article 253 specifications or be FIA homologated under the latest international regulations and accompanied by original certification documentation (older homologated cages are not all valid for a newly built vehicle).

See www.fia.com, under Sport, Regulations, International Sporting Code, Appendix J (at bottom), Article 253, Article 8.

12.3.2.4 Alternate material to CDS (Cold Drawn Seamless):

Although FIA Article 253.8.3.3 specifies the safety cage material as CDS (Cold Drawn Seamless), DOM (Drawn Over Mandrel) tubing may be used as an alternate material in respect to the following: Main roll bar, front roll bar, lateral roll bars, lateral half roll bars, their connections (drawings 253-1-3) and one continuous door bar per side will be at least 1.75"x.095". All other parts of the safety cage will be at least 1.5" x 0.095".

12.3.2.5 Approval of Safety cages:

A material certificate or an original sales receipt detailing the material used to fabricate the safety cage must be presented. For every tube size used in the fabrication of the safety cage, an unpainted sample section, 45cm long and bent 60 degrees must be presented when the cage is documented in the vehicle's log book.

12.3.2.6 Protective Padding:

Where the occupants' bodies could come into contact with the safety cage, flame retardant padding must be provided for protection.

Where the occupants' crash helmets could come into contact with the safety cage, the padding must comply with FIA standard 8857 2001, type A (see technical list n 23 "Roll Cage Padding Homologated by the FIA") or SFI 45.1.

12.3.3 Safety Harness.

12.3.3.1 Five or six point safety harness of unmodified proprietary manufacture, meeting the specifications below, shall be fitted for both crew members:

- (a) FIA Standard 8853/98
- (b) SFI 16.1
- (c) SFI 16.5

FIA homologated harness sets must not be used in competition after the expiration date on the label affixed to the harnesses. SFI licensed harness sets must have a date of manufacture label that is no older than two years.

Note: It is not permitted to mix parts of seat belts. Only complete sets may be used.

12.3.3.2 The material of all straps shall be in new or perfect condition. The belts must be equipped with turn buckle or push button release systems.

12.3.3.3 The lap belt and crotch straps should not pass over the sides of the seat, but through the seat in order to wrap and hold the pelvic region over the greatest possible surface.

The lap straps must fit tightly in the bend between the pelvic crest and the upper thigh. Under no circumstances may they be worn over the region of the abdomen.

Care must be taken that the straps cannot be damaged through chafing against sharp edges.

12.3.3.4 In all cases, it is most preferable that safety harnesses be installed on the anchorage points of the vehicle. The recommended geometrical locations of the anchorage points are shown in drawing 253-42. It is prohibited for the seat belts to be anchored to the seats or their supports.

12.3.3.5 The shoulder harness shall be a two-strap over-the-shoulder type. ("H" type configuration is permitted.) In the downwards direction, the shoulder straps must be directed towards the rear and must be installed in such a way that they do not make an angle of more than 45° to the horizontal from the upper rim of the backrest, although it is recommended that this angle should not exceed 10°. The maximum angles in relation to the centre-line of the seat are 20° divergent or convergent. (see diagram 253-42) Anchorage points creating a higher angle to the horizontal must not be used unless the seat meets the requirements of the FIA standard. (If the seat does not provide lateral restraint, the mounting point on the vehicle structure shall be a minimum of 50 cm behind the seat back when measured along the belt.)

12.3.3.6 If the manufacturer provides for safety wiring the locking bale to prevent accidental unfastening of the belts from their anchorage points, then it shall be necessary for the all such components to be safety wired.

12.3.3.7 The minimum acceptable size and grade of bolt used in the mounting of all belts and harnesses shall be 7/16 inch UNF, SAE grade 8, or, preferably, M12 8.8. When mounted, the bolts should work in shear and not in tension.

- 12.3.3.8 If installation on the series anchorage points is impossible for the shoulder and/or crotch straps, new anchorage points must be installed on the shell or the chassis, as near as possible to the centre-line of the rear wheels for the shoulder straps. The shoulder straps may also be fixed to the safety rollcage or to a reinforcement bar by means of a loop, and may also be fixed to the top anchorage points of the rear belts, or be fixed or leaning on a transversal reinforcement welded to the backstays of the rollbar. In this case, the use of a transversal reinforcement is subject to the following conditions:

The transversal reinforcement shall be a tube measuring at least 38 (1.5") mm x 2.5 mm or 40 (1.6") mm x 2 mm, made from cold drawn seamless carbon steel, with a minimum tensile strength of 350 N/mm².

The height of this reinforcement must be such that the shoulder straps, towards the rear, are directed downwards with an angle of between 10° and 45° to the horizontal from the rim of the backrest, an angle of 10° being recommended.

The straps may be attached by looping or by screws, but in the latter case an insert must be welded for each mounting point (see drawings 253-17C and 253-53 for the dimensions). These inserts will be positioned in the reinforcement tube and the straps will be attached to them using bolts of M12 8.8 or 7/16 UNF specification.

Each anchorage point must be able to withstand a load of 1470 daN, or 720 daN for the crotch straps. In the case of one anchorage point for two straps, the load considered will be equal to the sum of the required loads.

For each new anchorage point created, a steel reinforcement plate with a surface area of at least 40 cm² and a thickness of at least 3 mm must be used.

- 12.3.3.9 Principles of mounting to the chassis/monocoque:

- General mounting system: see drawing 253-43.
- Shoulder strap mounting: see drawing 253-44.
- Crotch strap mounting: see drawing 253-45.

- 12.3.3.10 A safety harness must be used in its homologation configuration without any modifications or removal of parts, and in conformity with the manufacturer's instructions. The effectiveness and longevity of safety belts are directly related to the manner in which they are installed, used and maintained. The belts must be replaced after every severe collision, and whenever the webbing is cut, frayed or weakened due to the actions of chemicals or sunlight. They must also be replaced if metal parts or buckles are bent, deformed or rusted. Any harness which does not function perfectly must be replaced.

12.3.4 Fire extinguishers.

- 12.3.4.1 One fire extinguisher with a minimum UL rating of 10 BC or two, each with a minimum rating of 5 BC, must be installed inside the passenger compartment. During installation, consideration must be given to quick release and security of attachment. One fire extinguisher must be located within easy reach of the Driver or Co-Driver when seated.
- 12.3.4.2 It is strongly recommended that Halon or a similar gaseous extinguishant be used. If a dry powder unit is used, the unit should be shaken or rapped sharply at frequent intervals to reduce the chance of the powder compacting.
- 12.3.4.3 Evidence must be produced that the fire extinguisher has been purchased or recharged within the preceding two years.
- 12.3.4.4 It is highly recommended that all vehicles comply with the FIA article 253.7 (extinguishers - Extinguishing Systems).
- 12.3.4.5 A fire extinguisher label (available through CARS) must be placed on the outside of the vehicle, on a non-glass surface, at the nearest point of access to a fire extinguisher.

12.3.5 First aid kit.

A comprehensive first aid kit shall be carried in the passenger compartment. The first aid kit must include:

- 10 Cleansing Wipes
- 25 Adhesive Bandages 1.9cmx7.5cm
- Knuckle Bandages
- 4 Fingertip Bandages
- 2 Gauze Pads 5cmx5cm
- 3 Gauze Pads 7.5cmx7.5cm
- 3 Gauze Pads 10cmx10cm

- 1 Elastic Gauze Bandage 5cm x 4.5m
- 1 Elastic Gauze Bandage 7.5cm x 4.5m
- 1 Pressure Bandage 10cmx10cm
- 1 Abdominal Pad 12cmx22cm
- 2 Oval Eye Pads
- 10 Butterfly Closures
- 6 Cotton Tip Applicators
- 1 Tongue Depressor
- 2 pair Vinyl Gloves
- 1 pair Scissors 14cm
- 1 First Aid Pocket guide
- 1 Adhesive Tape 2.5cm x 4.5m
- 1 pair Tweezers
- 1 Cold Compress
- 3 Triangular Bandages
- 2 Space Blankets

In addition, it is recommended to carry a CPR mask, if the competitor is trained in its use.

The first aid kit must be easily accessible, clearly identified and the complete kit easily/quickly removable by hand. It is recommended that the first aid kit be accessible from both sides of the car and from the seated position.

A first-aid kit label (available through CARS) must be placed on the outside of the vehicle, on a non-glass surface, at the nearest point of access to a first-aid kit.

12.3.6 Warning devices.

Three self-supporting, light-reflecting, daylight-visible triangular warning devices of a minimum size of 30 cm per side shall be carried in the vehicle. One of which must be located within easy reach of the Driver or Co-Driver when seated.

12.3.7 Batteries.

12.3.7.1 Batteries must be securely mounted.

12.3.7.2 If removed from the original location, all batteries shall be mounted inside covered, non-conductive boxes.

12.3.7.3 If mounted inside the passenger compartment, batteries shall be those that are completely sealed or so designed or modified to prevent acid spillage.

~~12.3.7.4 Batteries in Production Class cars must remain in their original location.~~

12.3.8 General circuit breaker.

12.3.8.1 It is strongly recommended that a spark-proof general circuit breaker with the capability of disconnecting all electrical circuits shall be mounted in the passenger compartment. (The integrity of a fuel injection computer may be protected by supplementary wiring.)

12.3.8.2 The location of the circuit breaker shall be that which makes it easily operable by either crew member or by persons outside the vehicle through either front door.

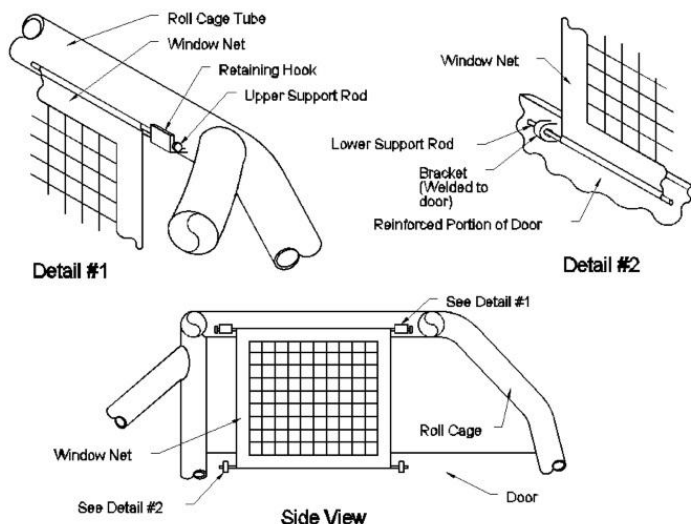
12.3.8.3 The location of the circuit breaker shall be marked with a label showing a red spark in a white-edged blue triangle with a base length of at least 12 cm.

12.3.9 Windows.

12.3.9.1 The windshield shall be laminated safety glass.

12.3.9.2 In all classes it is permitted and encouraged to replace electric winders with manual ones. In all cases, the competitor must be able to describe to the satisfaction of the event scrutineer the ability to escape from the car with the doors closed.

12.3.9.3 Windows in the driver and co-driver doors must not be rolled down more than 2.5 cm during stages. Window safety nets must be used in lieu of having windows rolled-up during stages. (See illustration for proper window net installation). It is highly recommended that all window nets meet FIA article 253.11



12.3.9.4 The use of translucent and colourless anti-shatter films is highly recommended in side and rear windows. The use of silvered or tinted anti-shatter films is also permitted. In all cases, the maximum tint shall be 50%.

12.3.9.5 For open, group 2 and group 5 vehicles, it is permissible to replace glass side windows with Lexan of like or greater thickness than the original glass. However, competitors must be able to display to the satisfaction of the event scrutineer that the mounting of the substitute windows will allow both emergency escape from inside the car and access by rescue from the outside of the car.

12.3.10 Mud flaps.

Mud flaps are required on all rear wheels and driving wheels.

12.3.11 Fuel, fuel tanks and lines.

12.3.11.1 Only unleaded fuels are allowed. Leaded fuel and lead additives are expressly prohibited.

12.3.11.2 A fuel-resistant and fire-retardant plate or shield is required between the passenger compartment and the compartment or area in which the fuel tank is located.

12.3.11.3 The original fuel tank may only be replaced by an FIA- or SFI- approved fuel cell provided that:

- (a) The original fuel tank is removed.
- (b) The fuel cell is properly vented to outside the vehicle from the compartment in which it is located.
- (c) The original fuel filler opening is sealed, if not used for the fuel cell.
- (d) Should the fuel cell and its filler be located in the luggage compartment, an outlet must be provided for fuel spilled in the compartment.
- (e) Where fuel cells are installed in the passenger compartment of vehicles such as "hatchback" variants, 12.3.11.2 above applies if the fuel cell filler is located in the passenger compartment.

12.3.11.4 Supplementary fuel tanks are not permitted.

12.3.11.5 If fuel lines are re-routed through the passenger compartment, they shall be in compliance with the following:

- (a) Shall incorporate a metallic casing. (If the metallic casing is not exterior to the line, a verifiable sample must be presented at scrutineering.)
- (b) Shall have a minimum of 200 psi rating. If fuel lines are routed through the passenger compartment by the manufacturer, it is recommended that they be in compliance with this section.

12.3.11.6 Fuel pumps shall be isolated from the driver/co-driver by a fireproof metal bulkhead.

12.3.12 Seats and seat mountings.

12.3.12.1 Seats:

- (a) The use of hinged-back and OEM seats is prohibited.
- (b) All the occupants' seats must be homologated by the FIA standards 8855-1999 or 8862-2009, and not modified.
- (c) It is recommended that seats be replaced after 5 years from the date of manufacture. Seats older than 10 years from date of manufacture must be replaced. Effective January 1, 2014, for seats in compliance with FIA standard 8855-1999, the limit for use is 5 years from the date of manufacture indicated on the mandatory label. An extension of 2 further years may be authorised by the manufacturer and must be indicated by an additional label. For seats in compliance with FIA standard 8862-2009, the limit for use is 10 years from the year of manufacture.

12.3.12.2 Seat Mounting:

- (a) Seats must be such that they are securely attached to the floor of the vehicle in such a manner as to prevent the movement of the seat in case of an accident. All seats must be mounted as per FIA Appendix J, Article 253, Article 16: Seats, Anchorage points and Supports. End plates may be fully welded instead of bolted.
- (b) Seat mounting of cars log-booked before June 1, 2011 are subject to the approval of the scrutineer.

12.3.13 Towing eyes.

Towing eyes shall be attached to the front and rear of the vehicle and painted in yellow, red or orange.

12.3.14 Loose articles.

All articles which could be dangerous if left loose must be securely restrained.

12.3.15 Door panels.

Inside door panels are required to provide protection from metal edges.

12.3.16 Tow Rope.

All vehicles must carry a tow rope or winch with cable. All parts of the tow rope must be within the competition vehicle at all times while the tow rope is not in use.

12.3.17 Roofs.

Movable metal sunroofs and/or roof panels must be fixed in the closed position. Sunroofs and/or roof panels of any other material must be replaced with metal and must be fixed in the closed position. The finished work must be of equal or greater strength than the permanent roof.

12.3.18 Supplemental & Passive Restraints.

Airbags and their associated equipment must be disabled or removed during competition in order to eliminate the possibility of the airbag inflating accidentally. It is recommended that passive restraint systems be disabled.

12.3.19 Ground Clearance

All parts of the vehicle, other than the tires, must have a minimum of 5.0 cm clearance from a level road surface.

12.3.20 Power Door Locks

For all classes, it is recommended that power door locks be rendered inoperative and replaced with manually operated mechanisms.

12.3.21 Steering Locking Device

For all classes, it is recommended that any steering locking device be rendered inoperative.

12.3.22 Camera and Camera Mounts

Camera mounts and their attachment to the vehicle shall be of a safe and secure design which would prevent either driver from being able to strike any part of the mount. As well, the camera shall be secured at a minimum of two different points and neither attachment may be elastic or plastic.

12.3.23 Belt Cutters

One or more belt cutters must be carried in the vehicle within reach of both driver and co-driver while safety harnesses are being worn. The seat belt cutter must be designed specifically for cutting seat belts. It is strongly recommended that one or more belt cutters be carried in the vehicle within reach of both driver and co-driver while safety harnesses are in place.

12.3.24 Spill Kit

All competition cars must carry a spill kit consisting of at least: a minimum of 2 -15" x 19" (standard) absorbent pads, 1- 3" X 48" Hydrocarbon sock. All items will be contained in a heavy duty plastic bag that is re-sealable.

12.4 General Regulations

12.4.1 Bodywork.

12.4.1.1 Bodywork must be without visible damage or perforation from corrosion which would detract from the good appearance of the vehicle.

12.4.1.2 The general shape and location of bumpers may not be changed.

12.4.1.3 The original headlight bezels or trim may not be removed.

12.4.1.4 The original radiator grillwork may not be removed.

12.4.1.5 If original exterior trim is removed, all mounting holes must be completely filled.

12.4.1.6 Paintwork must be finished and of neat appearance. Primer paint is not acceptable.

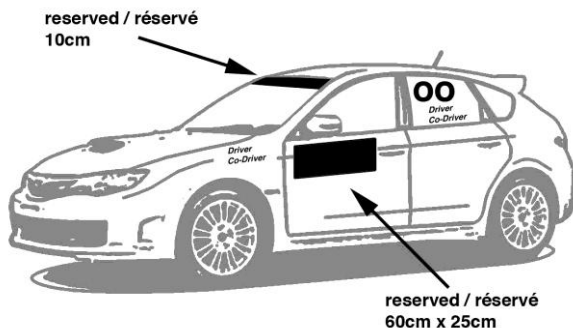
12.4.1.7 The name of the driver and the co-driver must appear either on both front wings or on the rear side windows of the car as shown in the diagram. The national or provincial flag of each crew member must appear adjacent to the name.

When the drivers' names appear on the rear side windows, the letters of the driver's name must be white, 5 cm in height in upper and lower case Helvetica bold face. The letters of the names must be the same size.

When the drivers' names appear on the front wings, letters must be of a contrasting colour to the paintwork. Specifications on size and font style given above must be followed.

12.4.1.8 The top 10 cm of the windscreen is reserved for use by CARS and/or its sponsors. No other advertising is permitted anywhere on the windscreen. Competitors have the option of installing a solid black background in the top 10cm area. The sponsor's decal is installed over this background.

12.4.1.9 An area 60 cm wide by 25 cm high, starting with the lead edge of the doors and from the top of the door panel downwards shall be reserved for exclusive use for the installation of the CARS door panels.



12.4.2 Lights.

12.4.2.1 Original headlights may not be changed or removed. However, the frontal glass, reflector and bulbs are free, provided they are in compliance with the legal requirements of the province or state of registration.

- 12.4.2.2 A headlight shall be considered as any lighting device throwing a beam toward the front of the vehicle (dipped-beam, long range lamp, anti-fog lamp). Auxiliary headlights may be fitted, provided the number is even. These lights may be fitted into the bumpers, radiator grillwork or the front part of the bodywork, provided that such openings as needed in this case are completely filled by the lights fitted.
- 12.4.2.3 It must be possible to turn off all high-beam headlights and auxiliary lights with a single switch, which must leave the low-beam headlights functioning.
- 12.4.2.4 It must not be possible to operate any fog lights fitted without the front marker lights and tail lights operating.
- 12.4.2.5 Auxiliary reversing lights may be fitted. They may be embedded in the rear bodywork if necessary.
- 12.4.2.6 All reversing lights may only switch on by engaging reverse gear.
- 12.4.2.7 It is not permitted to fit any device which can alter the normal functioning of the brake lights.
- 12.4.2.8 The mounting of maneuverable search lights is prohibited.

12.4.3 Tire studs.

Studs or other hard material devices inserted into the tire are not permitted, except as follows:

At regional events, taking place in a province which permits studded tires for street use, the CARS region which sanctions the event may allow the use of street-legal studded tires. For dual status events, competitors entering the national portion are still not permitted to use studded tires, even if they are also entered in the regional portion.

12.4.4 Exhaust system.

The maximum permitted noise level from the exhaust system shall be 86 dbA with the engine idling at 3500 RPM, measured from a distance of 15 meters.

Unless a car was legally registered before a catalytic converter was required in Canada, then a functioning catalytic converter must be retained or installed. (Burden of proof that a catalytic converter is not required for a particular vehicle rests with the competitor and should be furnished to the CARS technical director so that an exemption waiver may be added to the vehicle log book.)

12.4.5 Documentation.

The following documentation shall be carried in the vehicle at all times:

- (a) Vehicle registration.
- (b) Proof of third party liability insurance covering the entered vehicle.

12.4.6 Vehicle numbers.

All competing vehicles in stage events shall be identified with the vehicle number in five locations: one on the co-driver's side of the windshield banner, one on each of the door panels and one on each of the rear side window glass. (see diagram) These numbers will be supplied to competitors by the event organizer.

12.4.7 U.S. vehicles.

Vehicles registered in any of the United States of America, where the driver is competing on CARS entry permit (NRR 11.1.10), shall be acceptable to compete in Canadian events provided that they meet the requirements of current RA/NASA Rally Regulations. Entrants whose cars are entered under this provision must be able to produce, upon request by CARS or event officials, a copy of the relevant Rally Regulations. Failure to produce these regulations may result in vehicle being judged according to CARS regulations.

US vehicles entered in Canadian rallies shall be classified by CARS vehicle class regulations.

12.4.8 Vehicle Eligibility

The vehicle must be based on a model built by a recognized manufacturer. The manufacturer must be listed in the NADA Official Used Car Guide. It is the intent of these rules that all vehicles be based on production vehicles. Eligibility is restricted to street-licensed, closed-bodied vehicles. Non-production based vehicles built from the ground up, are explicitly prohibited.

12.4.9 Tire Coverage

Tires must be fully covered when viewed from above.

12.5 Open 4WD

12.5.1 Definition.

Any AWD or 4WD vehicle which meets the requirements of NRR 12.1 to 12.4, and 12.5.2 to 12.5.7. This includes vehicles of series or limited production, which are modified beyond what is permitted in Production 4WD. Innovation and modifications within the rules are encouraged.

12.5.1.1 12.5.1.1 FIA Vehicles

Vehicles which do not comply with NRR 12.5.1, but do comply with FIA regulations for Group A rally cars (including World Rally Cars) are permitted to compete in open 4WD Class, but shall not be eligible to score points for the Canadian Rally Championship.

~~12.5.1.2 2WD Vehicles (Effective January 1, 2015)~~

~~Vehicles which comply with NRR 12.6.1.1 and 12.6.4 to 12.6.6.1, but with engine displacements greater than allowed in NRR 12.6.2.1, and/or forced induction air inlet orifices greater than allowed in NRR 12.6.3 are permitted to compete in Open 4WD Class. These vehicles shall also be eligible to score points for the Canadian Rally Championship in Open 4WD Class. For these vehicles, NRR 12.5.2 to 12.5.7 do not apply.~~

12.5.2 Engine

The engine is unrestricted, but must be derived from the product line offered by the vehicle manufacturer.

12.5.2.1 Engine Displacement

The maximum displacement limit for both normally aspirated and forced induction engines is 2500cc. Rotary engines are restricted to twin rotors. Peripheral porting is not permitted.

12.5.3 Turbocharger/Supercharger Restrictions.

Forced induction engines must have an air inlet orifice of 34mm diameter or less. If the forced induction is equipped with a larger inlet, the use of a restrictor is required. It must have a minimum width (parallel to the direction of the airflow) of three (3) mm and be placed within 50 mm of the compressor blades.

If a car is fitted with multiple induction systems, then the total area of the two restrictors cannot exceed the area of the restrictor listed above. Competitors must have in place a mechanism to allow the induction system to be sealed by the use of wire and CARS seals. With the wire and seal in place, it must be impossible to access the restrictor without removing the wire and seal. The wire and seal cannot be installed without a detailed examination of the restrictor and, without the seal being in place, competitors must be prepared to dismantle the induction system to allow for verification of compliance with the rule above

12.5.4 Bodywork.

12.5.4.1 Except for 12.5.4.2, the modification, reinforcement, substitution, addition or deletion of parts and components is permitted without restriction.

12.5.4.2 Exterior bodywork (i.e.: all components licked by the airstream) must be visually similar to the original item. Except for front doors and roofs, bolt-on body pieces may be constructed of an alternate material. A and B pillars must remain original. The original floor pan and firewall must remain and may be modified only to the extent necessary to accommodate allowed alternate components. Roof mounted vents are allowed.

12.5.5 Mechanical Components.

Brakes, carburettor/injection, transmission, suspension, cooling, final drive ratio and type, clutch, pressure plate and flywheel are unrestricted.

12.5.6 Electronic Controls

No type or form of electronic control is permitted for the following components: Suspension, Braking, Gear change/clutch, front and rear differentials. Simple engine cut operating during a mechanically activated gear change is permitted

12.5.6.1 The use of an unassisted manual sequential gear change mechanisms is allowed.

12.5.7 Weight

For Open 4WD class vehicles, the absolute minimum real weight of the car is set at 2900 pounds. This is the real weight of the car, without fuel, driver, co-driver and their personal equipment. In case of a dispute

during weighing the full equipment of the driver and co-driver will be removed, including their helmets. Headphones external to the helmet, tools, spare parts, including tires and wheels and championship recording equipment shall be left in the car. At no time during the event may a car weigh less than the absolute minimum real weight stated in these regulations. The use of securely fixed ballast to complete the weight of the car is permitted.

12.6 Open 2WD

12.6.1 Definition.

Any 2WD vehicle which meets the requirements of NRR 12.1 to 12.4, and 12.6.2 to 12.6.6. This includes vehicles of series or limited production, which are modified beyond what is permitted in Production 2WD. Innovation and modifications within the rules are encouraged.

~~12.6.1.1 (Effective January 1, 2015)~~

~~2WD vehicles with engine displacement greater than permitted in 12.6.2.1 and/or forced induction air inlet orifices greater than permitted in 12.6.3 will be allowed to compete at national rallies however will be classified as Open class vehicles. These vehicles must comply with all other aspects of the Open 2WD regulations (ie NRR 12.6.4 to 12.6.6) to be eligible. See NRR 12.5.1.2.~~

12.6.2 Engine and weight.

The engine is unrestricted, but must be derived from the product line offered by the vehicle manufacturer.

The regulated weight is the real weight of the car, without fuel, driver, co-driver and their personal equipment. In case of a dispute during weighing the full equipment of the driver and co-driver will be removed, including their helmets.

Headphones external to the helmet, tools, spare parts, including tires and wheels and championship recording equipment shall be left in the car. At no time during the event may a car weigh less than the absolute minimum real weight stated in these regulations. The use of securely fixed ballast to complete the weight of the car is permitted.

~~12.6.2.1 Engine Displacement and weight.~~

~~Vehicles must meet the following minimum weight, dependant on the engine displacement and use of forced induction.~~

~~Naturally Aspirated Engines:~~

- ~~• Up to 2500cc –No Minimum Vehicle Weight~~
- ~~• 2501cc to 3200cc –Vehicle Minimum Weight 2700lbs~~

~~Forced Induction Engines:~~

- ~~• Restricted Induction Up to 2000cc –No Minimum Vehicle Weight~~
- ~~• Restricted Induction 2001cc to 2500cc –Vehicle Minimum Weight 2700lbs~~
- ~~• Un-Restricted Induction Up to 2500cc –Vehicle Minimum Weight 2900lbs~~

~~Rotary Engines:~~

- ~~• restricted to twin rotors. Peripheral porting is not permitted –No Minimum Vehicle Weight~~
- ~~• The maximum displacement limit for normally aspirated engines is 3000cc and is also limited to a maximum of 5 cylinders.~~
- ~~• The maximum displacement limit for forced induction engines is 2000cc.~~
- ~~• Rotary engines are restricted to twin rotors. Peripheral porting is not permitted.~~

12.6.3 Turbocharger/Supercharger Restrictions.

Forced induction engines must have an air inlet orifice of 32mm diameter or less.

If the forced induction is equipped with a larger inlet, the use of a restrictor is required. It must have a minimum width (parallel to the direction of the airflow) of three (3) mm and be placed within 50 mm of the compressor blades.

If a car is fitted with multiple induction systems, then the total area of the two restrictors cannot exceed the area of the restrictor listed above. Competitors must have in place a mechanism to allow the induction system to be sealed by the use of wire and CARS seals. With the wire and seal in place, it must be impossible to access the restrictor without removing the wire and seal. The wire and seal cannot be installed without a detailed examination of the restrictor and without the seal being in place, competitors

must be prepared to dismantle the induction system to allow for verification of compliance with the rule above.

12.6.4 Bodywork.

12.6.4.1 Except for 12.6.4.2, the modification, reinforcement, substitution, addition or deletion of parts and components is permitted without restriction.

12.6.4.2 Exterior bodywork (i.e.: all components licked by the airstream) must be visually similar to the original item. Except for front doors and roofs, bolt-on body pieces may be constructed of an alternate material. A and B pillars must remain original. The original floor pan and firewall must remain and may be modified only to the extent necessary to accommodate allowed alternate components. Roof mounted vents are allowed.

12.6.5 Mechanical Components.

Brakes, carburettor/injection, transmission, suspension, cooling, final drive ratio and type, clutch, pressure plate and flywheel are unrestricted.

12.6.6 Electronic Controls

No type or form of electronic control is permitted for the following components: Suspension, Braking, Gear change/clutch, front and rear differentials. Simple engine cut operating during a mechanically activated gear change is permitted

12.6.6.1 The use of an unassisted manual sequential gear change mechanisms is allowed.

12.7 Group Five (Effective until December 31st, 2014)

Note: This section rescinded on December 31, 2014

12.7.1 Definition.

Vehicles must be 2-wheel drive, sold globally in minimum quantities of 1000. Drive configuration must remain as originally manufactured (e.g. front engine, front drive).

12.7.2 Engine.

The engine is unrestricted, but must be derived from the product line offered by the vehicle manufacturer, either as production in any vehicle or high performance replacement.

12.7.3 Adjusted Displacement.

Adjusted displacement must be no greater than 5100cc using the following multipliers:

- | | |
|---------------------------------|-----|
| • Rotary engines | 1.8 |
| • Turbo or supercharged engines | 1.7 |
| • Pushrod engines | 0.8 |

12.7.4 Bodywork.

12.7.4.1 Except for 12.7.4.2 and 12.7.4.3, the modification, reinforcement, substitution, addition or deletion of parts and components is permitted without restriction, provided the vehicle complies with the safety and general regulations.

12.7.4.2 Exterior pieces (i.e.: all components licked by the airstream) must be visually similar to the original item, including bumpers. Except for doors and roofs, bolt-on body pieces may be constructed of an alternate material. A and B pillars must remain original. The original floor pan and firewall must remain and may be modified only to the extent necessary to accommodate allowed alternate components. In all cases, there must be a complete metal separation between the passenger compartment and engine. Roof mounted cooling vents are allowed.

12.7.4.3 Fenders may be modified to the extent necessary to provide tire clearance. Fender flares may be added. Installation of one-piece flared fenders or quarter panels is prohibited.

12.7.5 Mechanical Components.

Brakes, carburettor/injection, transmission, suspension, cooling, final drive ratio and type, clutch, pressure plate and flywheel are unrestricted, except that no form of electronic control is permitted for sequential gear change/clutch (except for automatic transmissions) or differential.

12.7.6 Wheels.

Wheel diameter and width are unrestricted.

12.8 Production 2WD and 4WD**12.8.1 Definition.**

Vehicles available through normal marketing channels in Canada and/or US, with limited modifications in order to make them more suitable for competition with respect to safety and reliability. Modifications, unless allowed under NRR 12.8.2 to 12.8.15, are not permitted.

12.8.1.1 Minimum production or importation.

There must have been a minimum of 100 units of the specific make and model and of a specific model year commercially available in Canada or 1000 units in the United States. Commercially available means a new vehicle for the Domestic North-American market, sold by the manufacturer through its dealer network, in Canada and/or the U.S.

12.8.1.2 Original equipment (OEM).**Production 2WD:**

- Use of original equipment parts or components from other model variants and/or model years is not permitted.

Production 4WD:

- Original equipment parts or components from the same model and any year of manufacture are allowed.

12.8.1.3 Tolerances.

All adjustment-machining must be at the manufacturer's specification and/or within the manufacturer's specified tolerances.

12.8.2 Engine.**12.8.2.1 Over boring.**

Over boring for the use of oversize pistons is prohibited, unless it is within OEM specifications, and using OEM oversize pistons.

12.8.2.2 Motor Mount.

Motor mounts are free.

12.8.2.3 Engine Displacement.**Production 2WD:**

- The maximum displacement limit is 2500cc.
- The maximum displacement limit for forced induction engines is 1600cc.
- Rotary engines are restricted to twin rotors. Additional porting and forced induction is not permitted.

Production 4WD:

- The maximum displacement limit for both normally aspirated and forced induction engines is 2500cc.
- Rotary engines are restricted to twin rotors. Additional porting is not permitted.

12.8.3 Turbocharger/Supercharger Restrictions.**Production 2WD and Production 4WD**

Turbocharger and supercharger units must be OEM for the model.

Forced induction engines must have an air inlet orifice of 32mm diameter or less. If the forced induction is equipped with a larger inlet, the use of a restrictor is required. It must have a minimum width (parallel to the direction of the airflow) of three (3) mm and be placed within 50 mm of the compressor blades.

If a car is fitted with multiple induction systems, then the total area of the two restrictors cannot exceed the area of the restrictor listed above. Competitors must have in place a mechanism to allow the induction system to be sealed by the use of wire and CARS seals. With the wire and seal in place, it must be impossible to access the restrictor without removing the wire and seal. The wire and seal cannot be installed without a detailed examination of the restrictor and, without the seal being in place, competitors

must be prepared to dismantle the induction system to allow for verification of compliance with the rule above.

12.8.4 Body Work.

Weight reduction of *ORIGINAL* parts and components is permitted except for the restrictions listed below.

- Exterior bodywork (i.e. all components licked by the airstream) must be visually similar to the original item. The modifications derived from the fitting of supplementary accessories authorized in 12.8.14 (i.e. such as those necessitated by the addition of a windshield washer by drilling of a hole in the hood), will be allowed.
- No composite materials allowed.
- The main unibody must remain as OEM.
- All doors and front bumper beam must remain as OEM.
- Driver's and Co-driver's door structure must remain unaltered.
- No replacement of stock parts with magnesium or titanium components is allowed unless these came as OEM parts.
- All glass must stay as per original equipment or of equivalent OEM specifications.
- None of the normal elements of the interior cockpit bodywork (including the dashboard and the elements contained in or part of the dashboard) and none of the accessories normally mounted by the manufacturer on the lowest price model may be removed or replaced.
- Carpets, padding, liners, etc. may be removed or modified.
- Standard seat/shoulder belts may be removed.
- The glove box door may be removed/replaced.
- Seam welding the body work is permitted.
- The rear seat may be removed.
- Side, roof, pillar, door and rear mouldings may be removed or modified. (see NRR 12.3.15)
- Interior lighting may be removed or modified.
- Roof mounted vents are allowed.
- Center console may be removed or modified.

12.8.5 Mechanical Components

~~Brakes,~~ Suspension, differential, clutch, pressure plate and flywheel are free.

Brakes are free but must retain the OEM pedal box and equal number of master cylinders as OEM. OEM pedal box being the OEM brake pedal, location, mounting and actuation. An additional master cylinder for a hydraulic hand brake is acceptable.

12.8.5.1 Transmissions.

Production 2WD:

- The transmission installed must be OEM for the model and year of manufacture of the vehicle.

Production 4WD:

- Any transmission normally installed by the manufacturer for the model may be used, except that electronically controlled sequential transmissions are not allowed.

12.8.6 Electronic Controls

No type or form of electronic control is permitted for the following components: Suspension, Braking, Gear change/clutch, front and rear differentials. Simple engine cut operating during a mechanically activated gear change is permitted

12.8.7 Minimum weight.

Production 2WD:

- The minimum weight of the car is unrestricted.

Production 4WD:

- The absolute minimum real weight of the car is set at 3350 pounds. This is the real weight of the car, without fuel, driver, co-driver and their personal equipment. In case of a dispute during weighing the full equipment of the driver and co-driver will be removed, including their helmets. Headphones external to the helmet, tools, spare parts, including tires and wheels and championship recording equipment shall be left in the car.

At no time during the event may a car weigh less than the absolute minimum real weight stated in these regulations. The use of securely fixed ballast to complete the weight of the car is permitted. Exceptions to the minimum weight limit may be granted by the technical director for smaller and/or lower powered Production 4WD vehicles. This exemption is not intended to be applied broadly, but only to vehicles significantly disadvantaged by the normal weight limit. Examples would include a normally aspirated 4WD vehicle or those with smaller displacement forced induction motors. The minimum weight for exempted vehicles will be the listed curb weight for the model.

12.8.8 Induction.

The carburettor(s) or fuel injection normally installed by the manufacturer must be OEM for the model. The electronic control unit (ECU) for the injection is free. The elements which control the quantity of fuel fed into the engine may be modified, but not those which control the volume of air. Such alterations must not allow any additional air to be inducted to the engine (i.e., the removal of a vacuum hose from the air cleaner housing requires capping off the hole in the air cleaner housing).

12.8.8.1 For forced induction engines, the boost is free.

12.8.8.2 The intercooler(s) must remain OEM or OEM equivalent and mounted in the original mounting location(s).

12.8.8.3 The air filter housing and the air filter are free. All air entering the engine must pass through the air filter.

12.8.9 Exhaust.

The exhaust system is free, except that the stock exhaust manifold(s) must be retained. The pipe(s) must exit behind the driver and external to the body. A functioning catalytic converter must be retained or installed.

12.8.10 Fuel Cells and Fuel Lines.

Fuel cells, not to exceed the capacity of the stock fuel tank, or 70 litres (15 gallons), whichever is greater, are allowed to replace stock fuel tanks. Location is free. Fitting of such cells must conform to NRR 12.3.11.3. Rerouting of fuel lines is permitted, but they must conform to NRR 12.3.11.5.

12.8.11 Electrical Equipment.

12.8.11.1 Battery and Alternator.

The original battery and alternator may be replaced by another commercial automobile unit of equal or larger capacity. The location and make is free.

12.8.11.2 Lighting Devices.

Additional lighting is permitted, but must conform to NRR 12.4.2.

12.8.12 Wheel diameter and width are unrestricted. See NRR 12.4.9.

12.8.13 Cooling System is free.

12.8.14 Supplementary Accessories.

Supplementary accessories are authorised without restriction provided they have no influence whatsoever on the behaviour or the outside appearance of the car. Such items are those which concern the aesthetics or the inside comfort (lighting, heating, radio, etc.) or those enabling easier or safer driving of the car (extra windshield washer, etc.) provided they do not affect, even indirectly the performance of the engine, the steering, the transmission, the road holding or handling characteristics of the vehicle. The following is authorised:

- Nuts and bolts may be freely exchanged and locked by any means.
- The mounting of additional gauges, meters, etc.
- The horn may be changed or an extra one added, at the disposal of the co-driver if desired.
- Extra relays, switches, wiring, fuses and circuit breakers may be added to the electrics.
- The original accelerator cable may be replaced by another one whether supplied or not by the manufacturer.
- The luggage boot may be adapted for better accommodation of the equipment carried (straps for fixing of a tool box, additional spare wheel, etc.). The location and attachment system of the original spare wheel may be altered and "Space Saver" spares may be removed.

- Filler caps may be locked by any means.
- The steering wheel is free.
- The addition of protection for the engine sump, transmission, fuel tank and all fluid lines is permitted.
- Cruise control systems may be removed.
- Anti-theft systems may be removed.
- Any elements of the air conditioning system and related hardware may be removed.
- Replacement and removal of electric window mechanisms by wind-up window mechanisms is permitted. Removal of sun roof mechanisms is also permitted.
- Removal of standard radio/stereo systems is permitted.

12.8.15 Consumable Items.

The replacement of normally consumable items (oil filters, air filters, spark plugs, fan belts, etc.), by others of equivalent OEM specifications is permitted. Spark plug heat range is free.

12.9 Vehicle Log Books

- 12.9.1 A standard CARS Vehicle Log Book shall be issued for all vehicles. The log book shall remain with the vehicle, including changes of ownership.
- 12.9.2 Only one log book is issued to each vehicle (other than by way of extension or replacement) and the possession of two log books for one vehicle at one time shall be deemed a breach of the GCRs.
- 12.9.3 The log book is issued by the CARS Director of the host region or his designated representative.
- 12.9.4 A complete description of the vehicle is to be entered in the places provided. All changes of ownership of the vehicle must be recorded as provided.
- 12.9.5 At each event, the log book must be surrendered to the chief scrutineer at scrutineering with the signature of the driver/entrant for that event in the space provided.
- 12.9.6 During scrutineering, any deviations should be noted by the scrutineer and, at the conclusion of scrutineering the chief scrutineer should give all log books collected to the Steward(s).
- 12.9.7 If a car is protested during an event, the results of this protest must be noted in the log book by the Steward(s).
- 12.9.8 If, during an event, the vehicle is involved in an accident or is damaged due to mechanical failure, the damage is to be noted in the Vehicle Log Book by the scrutineer or the Steward(s).
- 12.9.9 It is the responsibility of the entrant to collect the log book at the end of the event.
- 12.9.10 SCCA/RA/NASA Vehicle Log Books are to be accepted at all CARS rallies.
- 12.9.11 Failure to present the Vehicle Log Book at scrutineering will result in a \$25.00 fine which must be paid in full before the offending entrant will be allowed to start the event.

13. ORGANIZATION OF SPECIAL STAGE RALLIES

13.1 Definitions and General Regulations

13.1.1 Special stages are portions of the route intended to test the skill of the driver and the performance and handling of the car. They are subject to the following conditions:

13.1.1.1 They are to be identified in the route instructions with distance for the Start Time Control, the Start Line, the Flying Finish and the Stop control.

13.1.1.2 Competitors are to start on separate minutes. For the first five drivers (or the number equalling the total of seed one drivers starting the event, provided that there are more than five) starting a stage in a Canadian Rally Championship event, an additional minute is to be provided: e.g. 2 minutes if the event is running on one minute spacing, three minutes if running on two minute spacing. (Organizers of regional championship events may also elect to follow this aspect of the regulation with approval of their region director.)

For Super Special stages within a rally, organizers may adopt special start order or start intervals for stage start separations if approval is given by the National Series Manager for Canadian championship events or the region director for regional championship events. However, such changes shall be outlined in the event's supplementary regulations.

13.1.1.3 All intersections in the stage are to be controlled.

13.1.1.4 The course is to be marked (preferably by use of barricades plus arrows) so that the proper route can be followed and no short cuts can be taken.

13.1.1.5 It is recommended that all stage starts be on level ground and/or on a downhill slope for the first 150 meters.

13.1.2 Permission to hold each of the special stages is to be obtained, in writing, from the proper authorities.

13.1.3 Insurance is to be obtained through CARS national office.

13.1.4 Before any competitor is allowed to start a special stage, the following conditions should be met and be maintained until all competing cars have completed the stage, or have withdrawn from the rally:

13.1.4.1 Communication should be established between the start and finish controls.

13.1.4.2 The course should be cleared and kept closed to all non-competing vehicles.

13.1.4.3 A fully equipped first aid vehicle manned by personnel of not less than senior St. John Ambulance certification should be present or on call at all times. It is recommended that a doctor be in attendance.

13.2 Choice of Route

13.2.1 Ideally, the average speed of the fastest competitor should be lower than 120 km/h. If a 120 or higher km/h average speed is achieved during a competition, then approval from CARS following a stage review (as detailed in NRR 13.8 below) must be obtained before the stage may be run in succeeding years. However, any stage which meets or exceeds a 132 km/h (120 +10%) average speed cannot be used in succeeding years. (Any stage which is run on a circuit designed for automobile racing is not subject to the above restrictions.)

13.2.2 Anything which creates a nuisance or inconvenience to anyone not connected with the rally should be avoided.

13.2.3 Twisting country roads should not be used for rally traffic that is travelling in both directions at the same time.

13.2.4 Organisers should notify police officials of the general rally route and the approximate times that competitors will be on this route.

13.2.5 Service stops of at least 20 minutes must be located not more than 150 kilometres apart except where circumstances forbid and the National Series Manager or the CARS Director from the host region authorises an extension. The distance (if in excess of 150 kilometres) must be stated in the supplementary regulations (as detailed in NRR 19.2.16, below).

13.3 Instructions

13.3.1 All instructions given to competitors which define the route or timing must be in writing.

13.3.2 Except where the organizer has received authorization from the CARS Board to follow the general prescriptions of the FIA, a set of instructions:

- (a) May only be given to competitors at the start or finish of a leg.
- (b) Must contain complete instructions for one or more legs.

13.3.3 A specific and readily identifiable object of a permanent nature must be described in the instructions as an odometer check. This check must be a minimum distance of 10 kilometres. The route prior to the odometer check must be defined in the instructions so as not to rely on accurate distance. No control may be located within an odometer check, except for observation controls (see NRR 16.7, below). An odometer check must be provided in each leg in which a different vehicle and/or odometer calibration was used to establish the route instruction distances.

13.3.4 Distances are to be statute to within plus or minus 2%.

13.3.5 All competitors are to receive identical route instructions.

13.3.6 When the event is a blind rally, route instructions are not to be issued so far in advance as to permit any competitor to practise any part of the route except the odometer check. However, route instructions are to be available at least three hours before the departure of the first vehicle.

13.3.7 The locations of rest, fuel and meal stops and the finish must be in the route instructions.

13.3.8 Telephone numbers of area hospitals, doctors, police and towing services, as well as the rally headquarters, must be shown in the route book.

13.3.9 All rally organizers are to provide route books with instructions presented in one of two standard formats as follows:

- (a) The number of the page is to be written in the upper right-hand corner.
- (b) The start of each stage must always be at the beginning of a new page.
- (c) The time of car "0" is to be included for every control identified in the route book.
- (d) For all blind rallies or where the organizer wishes to produce a traditional route book, no more than 10 instructions per 21.5 x 29 cm page in vertical form are allowed. It is suggested that the route distance between the first instruction on the second or subsequent pages of a stage be no less than 0.5 km from the last instruction on the previous page.

For reconnaissance or organizer supplied notes rallies and where the organizer wishes to produce a smaller sized route book (14 x 21.5 cm page), no more than 6 instructions from the first instruction on the second or subsequent pages of a stage be no less than 0.5 km from the last instruction on in vertical form is to be used. It is suggested that the route distance between the previous page.

For all organizer supplied route books, the back cover is to be a full sized OK/Red Cross with the Red Cross on the outside. (For the small sized route book, the back cover is to be folded so that, when assembled, the cover can easily be opened to its full size.)

- (e) The page is to consist of vertical columns from left to right containing at least the elapsed time between specific points, the cumulative distance from section or leg start, the non-cumulative distance between instructions, a tulip diagram and other clarifying information (such as visible signs, etc.). (See Diagrams).
- (f) Copy is to be clear black type on contrasting paper with stage instructions printed on white paper.
- (g) If there is a closed circuit on the route, then a map of the course must be provided.

13.3.10 Organizers are to indicate the legal speed limit for the first part of each transit section coming off of a stage. This information is to be located in the clarifying information column.

13.3.11 The minimum time allowance for any transit section is the time required to cover the section at 20% below the posted speed limit plus three minutes, or a total of five minutes, whichever is greater. If the transit includes a refuelling zone, then an additional five minutes must be included.

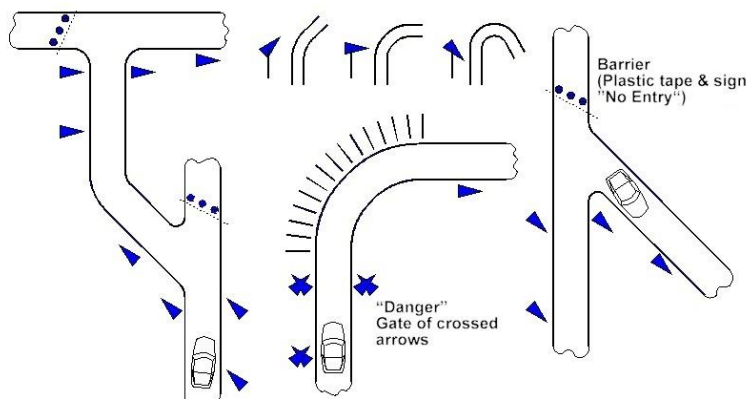
13.3.12 Organizers may use radar or other means to control speeding in transits. The penalties shall be as per NRR 17.2. Competitors in excess of 30 km/h. over the legal limit are to be disqualified. Organizers must use observation controls to inform any crew of a speeding violation.

13.3.13 Event organizers wishing to use organizer-supplied course notes are to request approval, by the CARS board through the CARS director for the host region. The request should include the method of preparation and the name of the author of the notes prior to publishing the event supplementary regulations. (See NRR 19.2.18.) Where circumstances permit, event organizers using course notes are encouraged to plan for a controlled competitor familiarization pass so competitors can familiarize themselves with the notes and how they relate to the stages to be used.

13.4 Arrowing

Double and triple cautions, are to be arrowed. Additional arrowing may be used at the discretion of the organiser. Where arrowing is used, the system should conform to the following:

- 13.4.1 One large advance warning arrow is placed ahead of a junction. It is usually placed about 100 meters ahead of the junction, but this can be more if approach speeds are very high or less if there are two junctions close together.
- 13.4.2 Two large arrows are to be placed at the junction to form a gate. These will normally, but not necessarily, be on the approach road to the junction, as shown on the sketches below. The important thing is that the gate of arrows should be readily visible to the driver.
- 13.4.3 One small vertical arrow should be placed after the junction to confirm that the correct route has been taken. The small arrows may also be used on their own to indicate "straight on" at junctions where there is no chance of a competitor taking the wrong turn.
- 13.4.4 Roads which are not to be used should be blocked or controlled as safely and completely as possible. A "no entry" sign and, if possible, plastic tape should be placed across the road.
- 13.4.5 Caution signs should be placed 50 to 100 meters before a hazard.
- 13.4.6 Angles of warning arrows and gate arrows should indicate the angle of the junction or bend, as shown in the sketch, but using one of three basic angles. The advance arrow should be on the opposite side of the road to the direction of the turn.



13.5 Chicanes

- 13.5.1 The use of artificial chicanes is permitted in rallies.
- 13.5.2 Chicanes may be of two types;
 - (a) Single element, designed to have the competitors leave the main road and then return, may be used at a delta or a road junction.
 - (b) Multiple element, designed to have the competitors negotiate the elements without leaving the road.
- 13.5.3 Chicanes should be manned and the marshals are deemed judges of fact as to whether a competitor struck a chicane element or not.
- 13.5.4 Entry for all chicanes shall be from the right so that the first chicane element shall be on the left side of the vehicle. In the case of multiple element chicanes, there shall be an even number of elements.
- 13.5.5 Penalties for striking an element of a chicane are as per NRR 17.2.
- 13.5.6 Caution signs should be placed 50 to 100 meters before a chicane as per NRR 13.5 on both sides of the road and the location distance and number of elements of the chicane must be identified in both event route book and event supplied notes.

13.6 Press Demonstration/Shakedown Stage

Organizers are permitted to schedule a shakedown stage or a press demonstration stage which is open to all competitors who wish to take part. Since this activity is conducted on a closed stage, all participants must sign waivers and all Organizers should arrange insurance coverage through CARS. In addition, all safety standards should be in place. For a Shakedown stage, all mandated safety equipment for both driver and co-driver must be worn. For Press Demonstration Stages, the driver must wear all mandated safety equipment and the passenger must wear a minimum of helmet and properly adjusted harnesses.

13.7 Advertising

- 13.7.1 All advertising or promotional material used in connection with competitions must state that the event is sanctioned by CARS. Advertising material must be in good taste and not be misleading or untruthful. The words national, international championship, etc., when used to describe an event or used in the title of an event must be approved by CARS. Prior to official and final results being issued by CARS, all advertising pertaining to results of an event, must contain the wording "subject to official confirmation."
- 13.7.2 Competitors who allow their names or photographs to be used in connection with misleading advertising shall be liable to disciplinary action from CARS.
- 13.7.3 Advertising is allowed on vehicles participating in any CARS sanctioned event, provided the advertising is in good taste and it does not conflict in any way with the recognition of the competition numbers by marshals and other officials.

13.8 120 km/h Stage Review

Any stage, as per NRR 13.2.1, which exceeds an average speed of 120 km/h during a rally is subject to a review by CARS to determine if the stage may be used in succeeding years. The review will include a physical review of the stage, consideration of possible changes to the structure of the stage as well as consideration that 120+ km/h average speeds may be acceptable for the stage. Based upon the review's findings, CARS may grant special status concerning for that stage for succeeding years.

14. TYPES OF ROUTE PRESENTATION

14.1 General

14.1.1 There are four ways of presenting route information:

- (a) Blind rallies are where the route is presented by distance referenced tulip diagrams with some explanatory notes to help guide competitors
- (b) Reconnaissance rallies are where the competitors have the opportunity to pass over the special stages of the event ahead of time to make detailed notes that would describe the roads and the manner in which to drive each part
- (c) Course notes are a detailed description of the special stages of the event which include hazards that are known at the time the notes were prepared (competitors may be given an opportunity to have a route familiarization passage over all or some of the stages). These notes can be produced by mechanical means.
- (d) Organizer supplied notes are where the organizer has someone prepare detailed notes that would describe the roads and provide a moderate amount of explanatory details to help guide competitors (competitors are given an opportunity to have a route familiarization passage over the stages).

14.2 Notification

Event Supplementary Regulations will identify the type of route presentation that will be used as well as details outlining the procedures to be followed including schedules for route familiarization passes, registration for recce and route reconnaissance.

14.3 Disclaimer

Course notes, organizer supplied notes and reconnaissance are not required to traverse the stages. All events will provide a traditional route book to all competitors (see NRR 13.3.9) Competitors must realize that the information contained in traditional route books, course notes and organizer supplied notes is a subjective matter, which cannot fully take into account the capabilities of the individual vehicles, the competitors or the prevailing conditions at the time of competition. The responsibility rests with the competitors to drive safely within their capabilities at all times. While effort is taken with the preparation and production of route book formats, no responsibility can be accepted for their accuracy. The event organizers, the author(s) of the route book formats, CARS or any other party involved in the administration of a stage, disclaim any and all liability for any incidents alleged to have resulted from or to be connected in any matter to the use of these route book formats.

14.4 Limitations of use

In adopting use of the traditional route book, organizer supplied notes or course notes, competitors acknowledge and agree to the following conditions of use:

- 14.4.1 They will be used without alteration or embellishment unless amendments are authorized by instructions received from event officials;
- 14.4.2 They will not be reproduced by any means for use by any other person;
- 14.4.3 They will not be used at any other time other than the specific event for which they were produced;
- 14.4.4 Pre-event practicing or testing on special stages is forbidden (see NRR 17.8)

14.5 Marking Allowances

To assist teams with the languages or systems used in the traditional route book, organizer supplied notes or course notes; markings will be allowed provided the markings specifically fall under the following definitions/types:

- 14.5.1 Competitors may underline or highlight the words.
- 14.5.2 Competitors may convert abbreviations to full words.
- 14.5.3 Competitors may add terms such as "Brake", "Caution", or other similar terms to alert drivers to changes in the character of the stage.
- 14.5.4 Competitors may add organizer supplied information.
- 14.5.5 Competitors may convert distances from meters to yards or mileage (tenths or hundredths), but must be converted consistently throughout the notes.
- 14.5.6 On stages that are run more than once, competitors may mark in their books, comments from the first running of the stage for use on repeat runs of the stage. When a notes familiarization pass is offered,

competitors may also mark their stage notes book(s) with comments from any source except reconnaissance done in violation of CARS rules.

- 14.5.7 To assist competitors that are accustomed to different languages, they may transpose specific information in the instructions (example: change a 1R to a 6R).

14.6 Inspection and Penalty

The Series Manager and the Steward(s) have the right to inspect any competitor's route book, organizer supplied notes, course notes, or other documents present in the vehicle at any time. The use of the route book, organizer supplied notes or course notes in violation of any of the above conditions will be considered using Pace Notes and will be penalized according to GCR 7.4.

15. TIMING AND SCORING

15.1 General

- 15.1.1 The standard timing system for stage rallies is described below. No other system is permitted unless specifically agreed to by the CARS Board.
- 15.1.1.1 All control watches are to be synchronised with official rally time which, in turn, is to be displayed ~~on the official notice board in the registration area.~~
- 15.1.1.2 The timing minute is from 00 to 59 seconds. On transport sections, timing is to the preceding whole minute and on special stages to the preceding second or tenth of a second (see NRR 15.12.1).
- 15.1.1.3 Times are to be affixed to the competitor timing cards. Competitors must run according to the times on their timing cards.
- 15.1.1.4 At all controls, the time of arrival (in hours and minutes) is the time of departure unless a later time of departure is recorded on the timing card by the control official.
- 15.1.1.5 The marshal records the car number of every competitor who checks in, or sufficient information to uniquely identify the car.
- 15.1.1.6 If a competitor believes that a marshal has misread his watch, he shall ask the marshal to recheck the time.
- (a) If the marshal agrees that he did misread his watch, he is to correct the time in on the timing card with the correct time in. He is to also note the corrected time in on his record sheet.
- (b) If the marshal does not agree that he misread his watch, the competitor shall base his calculations on the time in recorded.
- 15.1.1.7 If a competitor doubts the accuracy of a marshal's watch, he shall ask the marshal to record on his timing card, the time in requested by him. The marshal is to do so but is not alter the timing card. The competitor shall base his calculation on the time in recorded on his timing card.

15.2 Time Card

- 15.2.1 At the start of the rally, each crew is to be given a time card on which the time allowed to cover the distance between two consecutive time controls should appear. This card shall be handed in at the finish control of one section and replaced by a new one before the start of the next section. Several time cards may be issued together, bound in the form of a book, covering a section or leg of the rally. When conflicts exist between times supplied by the organizer on the time card and in the route book, the times on the timing cards shall take priority.
- 15.2.2 Each crew is solely responsible for its time card. The crew alone is responsible for submitting the time card at the different controls and for the accuracy of the entries. The crew is responsible for any entries made on the time card.
- 15.2.3 Therefore, it is up to the crew to submit its time card to the marshals at the correct time, and to check that the time is correctly entered.
- 15.2.4 Hours and minutes are to be shown thus: 00:01 - 24:00. Only the minutes which have elapsed will be counted. Throughout the rally, the official time will be that specified ~~on the official notice board at registration.~~
- 15.2.5 The time card must be available for inspection on demand, especially at the controls where it must be presented personally by a member of the crew for entry of times recorded.
- 15.2.6 Any correction or amendment made to the time card will result in exclusion, unless such correction or amendment has been approved by the appropriate marshal.
- 15.2.7 The absence of a stamp or signature from any passage control, or the absence of a time entry at a time control, or the failure to hand in the time card at each control (time, passage or regrouping), or at the finish, will result in exclusion.
- 15.2.8 The appropriate marshal is the only person allowed to enter the time on the time card, by hand or by means of a print-out device.
- 15.2.9 Any divergence between the times entered on the crew's time card and those entered on the official documents of the rally will form the subject of an inquiry by the Steward(s), who will deliver a final judgement.

15.3 Restart Order

After the initial start, as determined by the seeded draw and where a minimum of 10% of an event's total stage distance has been completed, restarts are determined by one of the following:

- 15.3.1 Each entry's provisional overall position, including penalties whenever possible, or by the order of arrival at that restart.
- 15.3.2 Each entry's provisional overall standing based on stage times only.
- 15.3.3 Each entry's total sum of each stage's overall placing less the worst position (with the entry with the lowest total sum starting first). For example:

Car	Stage 1	Stage 2	Stage 3	Total, less the worse	Restart
1	1	1	7	2	1
2	3	3	3	6	3
3	2	2	2	4	2

No matter which method above is used, assigned times must respect NRR 13.1.1.2.

The organiser, after approval by the Steward(s), may place competitors in a different restart order than their scores would normally have entitled them. Such decisions are not grounds for inquiry.

15.4 Control Procedure

- 15.4.1 The check-in procedure begins at the moment the car passes the time control area entry sign.
- 15.4.2 Between the area entry sign and the control, the crew is forbidden to stop for any reason or to drive at an abnormally slow speed.
- 15.4.3 The actual timing and entry of the time on the time card can only be carried out if the two crew members and the car are in the control area and within the immediate vicinity of the control unless it is physically impossible because of obstruction for the vehicle to reach the control then timing shall take place when a member of the crew presents himself to the control marshal.
- 15.4.4 The check-in time corresponds to the exact moment at which one of the crew members hands the time card to the appropriate marshal.
- 15.4.5 Then, either by hand or by means of a print-out device, the appropriate marshal marks on the card the actual time at which the card was handed in, and nothing else.
- 15.4.6 The target check-in time is the time obtained by adding the time allowed to complete the road section to the start time for this section, these times being expressed to the minute.
- 15.4.7 The crew will not incur any penalty for arriving in the control zone during the minute preceding checking-in before time if the car enters the control area during the target check-in minute or the minute preceding it.
- 15.4.8 The crew does not incur any penalty for lateness if the act of handing the card to the appropriate marshal takes place during the target check-in minute.

Example: A crew which is supposed to check in at a control at 18h58 shall be considered on time if the check-in takes place between 18h58:00 and 18h58:59.

- 15.4.9 Any difference between the actual check-in time and the target check-in time is penalised as follows:
 - (a) For late arrival: 10 seconds per minute or fraction of a minute
 - (b) For early arrival: 1 minute per minute or fraction of a minute.
- 15.4.10 At the discretion of the organiser, a crew which has been penalised for early arrival may be neutralised for the amount of time necessary for it to leave at the time originally envisaged.
- 15.4.11 At the time controls at the end of a leg, of a section, of a regroup, or at the end of the event, the organisers may authorise the crews to check in in advance without incurring any penalty, provided that these time controls are identified as main time controls in the route book. However, drivers must request their correct time in and this requested time shall be entered on the time card not the actual time of arrival.
- 15.4.12 If it is found that a crew has not observed the rules for the check-in procedure as defined above (especially by entering the control area more than a minute before the actual check-in time), the senior marshal at the control is to make this the subject of a written report to be sent by the organiser to the Steward(s), who will impose any appropriate sanction.

15.5 Timing

15.5.1 If the next road section does not start with a special stage, the check-in time entered on the time card shall constitute both the arrival time at the end of the road section and the starting time of the following one.

15.5.2 However, when a time control is followed by a start control for a special stage, the following procedure should be applied:

15.5.2.1 These two controls shall be included in a single control area, the signs of which shall be laid out as follows:

- (a) Yellow warning sign showing a clock face (beginning of area).
- (b) Red sign showing a clock face (time control) at a distance of approximately 25 m.
- (c) Red sign showing a flag (start of the special stage) at a distance of 50 to 200 m.
- (d) Finally, end of control sign (3 transversal stripes on a beige background) 25 m further on.

15.5.2.2 At the time control at the finish of a road section, the appropriate marshal is to enter on the time card on the one hand the check-in time of the crew and on the other, its provisional starting time for the start of the stage. If two or more crews check in on the same minute at a time control immediately prior to the start of a Special Stage, their provisional start times for that Special Stage should be in the order of their relative arrival times at the preceding time control. If the arrival times at the preceding time control are the same, then the times at the time control previous to that one should be taken into account and so on. There must be a minimum 2-minute gap to allow the crew to prepare for the start.

15.5.2.3 Immediately after checking-in at the time control, the competing car shall be driven to the start line of the special stage. The start marshal will check the time foreseen for the start of the stage on the timing card. He will then start the crew according to the procedure laid down (see NRR 15.12.4).

15.5.3 The exact elapsed time required between any two controls, in minutes, is stated in the route book. There are no target times for special stages. However, in the route book, each special stage will be assigned a "lateness" time for the purpose of calculating maximum permitted lateness (MPL) and for calculating the time out of the stage.

15.5.4 The time out from the time control at the end of a stage is normally the time of starting the stage plus the lateness time of the stage. However, if the time taken on the stage is longer than the lateness time for the stage, the time in hours and previous whole minutes shall be used.

Examples	A	B	C
Stage 1 start time	10:00	10:00	10:00
Stage 1 lateness time	(8)	(8)	(8)
Stage 1 finish time	10:05:28	10:08:32	10:12:10
Stage 1 penalty	5:28	8:32	12:10
Stage 1 time out	10:08	10:08	10:12

The target time for the next time control after a stage finish is the calculated time out of the stage added to the time given for the road section.

Examples	A	B	C
Stage 1 time out	10:08	10:08	10:12
Transit time	22 min	22 min	22 min
Target time Stage 2 start	10:30	10:30	10:34

15.5.5 The maximum permitted lateness for each leg of the rally is 30 minutes (or longer with the approval of the region rally director). One (1) minute of MPL will be accumulated for each minute of late arrival at time controls and for each minute taken in excess of the "lateness" time to traverse a special stage. MPL may not be reduced by early arrival at controls. ("Lateness" times shall be quite generous and will represent the time that the organiser(s) anticipates the slower vehicles will require to traverse a special stage.)

15.5.6 Marshals are not to attempt to judge whether a competitor is within his maximum permitted earliness or lateness. The marshal should issue and record times for each competitor who checks in.

15.5.7 Maximum permitted earliness (MPE) for each leg of the rally is 10 minutes. One (1) minute of MPE will be accumulated for each minute of early arrival at time controls and main time controls. MPE may not be reduced by late arrivals at controls.

15.6 Exclusion

Crews are bound, under pain of exclusion, to check in at all times in the correct sequence and in the direction of the rally route. It is also prohibited to re-enter the control area.

15.6.1 Any lateness exceeding 30 minutes per each section and/or leg of the rally will result in the exclusion of the crew by the organiser. In calculating such exclusion, the actual time and not the penalty time, (10 seconds per minute) shall apply.

- 15.6.2 Early arrival shall under no circumstances permit crews to reduce the lateness counting towards exclusion. However, penalties for early arrival at a time control shall not be taken into consideration when calculating time counting towards exclusion for exceeding maximum lateness.

Examples

Section A:	Target time	13:00:00
	Check-in time	13:10:xx

Penalty for late arrival = 10×10 seconds = 100 seconds = 1 min. 40 seconds
Lateness counting toward exclusion = 10 minutes.

Section B:	Target time	14:30:00
	Check-in time	14:25:xx

Penalty for early arrival = 5 minutes.
Lateness counting towards exclusion = 10 minutes. (Not compounded)

Section C:	Target time	16:20:00
	Check-in time	16:30:xx

Penalty for late arrival = 10×10 seconds = 100 seconds = 1 min. 40 seconds
Lateness counting towards exclusion = 10 additional minutes.

TOTAL SECTIONS A + B + C

Total penalties (for late and early arrivals):
1 min. 40 seconds + 5 minutes. + 1 min. 40 seconds = 8 minutes. 20 seconds.

Total lateness counting towards exclusion: $10 + 10 = 20$ minutes.

- 15.6.3 The exclusion time may be increased at any point by the Steward(s), upon the proposal of the organiser. The crews concerned shall be informed of this decision as soon as possible.
- 15.6.4 Exclusion for exceeding the maximum permitted lateness may only be announced at the end of a section or at the end of a leg.

15.7 Regrouping Controls

- 15.7.1 Regrouping areas may be set up along the route. Their entry and exit controls shall be subject to the general rules governing controls.
- 15.7.2 Inside a regrouping area, the engines may be started by means of an external battery. This battery must not then be taken on board the car.
- 15.7.3 The purpose of these regrouping areas will be to reduce the intervals which may have occurred between competing cars as a result of late arrivals and/or retirements. Thus, the starting time from the regrouping control and not its duration must be taken into account.

Example: 60 cars at the start of the rally (9:00) with a 20 min regrouping at the end of service #1 (11:00 with 11:20 out of regrouping)

- | | | |
|-----|---|--|
| (a) | Target time for the arrival of car #1 at the regrouping control: | 11:01 |
| | Target time for the arrival of car #60 at the regrouping control: | 12:00 |
| (b) | Actual arrival time of car #1 at the regrouping control: | 11:01 |
| | Actual arrival time of car #60 at the regrouping control: | 12:09 |
| | (6 cars have retired during this part of the rally) | |
| (c) | Starting time of car #1 | 11:21 |
| | Starting time of car #60 (12:20 - 6 retired vehicles) | 12:14 |
| (d) | The respective length of stopping time shall therefore have been: | |
| | Car #1 | 20 min |
| | Car #60 | 5 min (9 min target lateness + 6 retired + 5 stop time = 20) |

- 15.7.4 On their arrival at these regrouping controls, the crews will hand the appropriate marshal their time card book. The crews will receive instructions on their starting time. They must then drive their car immediately and directly to the Parc Fermé. The organisers may give them a new set of time cards either at the entrance or at the exit of the Parc Fermé.
- 15.7.5 After regrouping, the starting order should follow the general classification drawn up according to NRR.15.3. Otherwise, vehicles should start in the order in which they arrived at the regrouping in-control.

15.8 Service Parks

Service parks will be established in accordance with the following.

- 15.8.1 Service parks are to be indicated in the rally itinerary and route book with a Time Control at the entrance and exit. (The 25 m distance referred to in NRR 16.1.4 and 16.9.1 shall be reduced to 5 m).
- 15.8.2 The speed of cars in the service parks may not exceed 30-20 km/h. A lower limit may be specified in the event supplementary regulations.
- 15.8.3 During the event, fuel in the service park must be stored in the team's trailer or service vehicle.

15.9 Refueling

Refuelling of the competitor's vehicle is only permitted in the refuel zones designated by the Organizers in the route book.

- 15.9.1 These refuel zones will be established immediately after the exit from the service parks or regrouping controls. Their entry/exit should be marked by a blue fuel can symbol.
- 15.9.2 Any action inside a refuelling zone not directly involved in the refuelling of the competing vehicle is prohibited. Refueling zones are to be considered a Parc Fermé.
- 15.9.3 In all refuelling zones, a 5 km/h. speed limit will apply.
- 15.9.4 The responsibility of refuelling is incumbent on the competitor alone. However, service crews may fuel their cars.
- 15.9.5 Engines must be switched off throughout the refuelling operation.
- 15.9.6 It is recommended that the crew remains outside the car during refuelling; however, should they remain inside, the safety belts must be unfastened.
- 15.9.7 In the event of a breakdown, the car concerned may be pushed outside of the zone without incurring a penalty. An external battery may be used immediately after the exit of the zone.
- 15.9.8 A five minute time allowance for refuelling must be included within the target time for the following transit. See NRR 13.3.11.
- 15.9.9 An electric fuel pump for fuel transfer is strictly prohibited.

15.10 Route Obstruction / Loss of Stage Security

- 15.10.1 If a competitor blocks the route for succeeding competitors (e.g.: with a winch cable), he shall be subject to a minimum penalty of exclusion from the event.
- 15.10.2 Competitors coming upon circumstances which have breached the security of the stage shall make all efforts to notify event officials of the situation through the next radio location or the finish control.
- 15.10.3 Competitors coming upon a total road blockage shall make all efforts to ensure the safety of the area as per NRR 17.9. After securing the scene, competitors are to make all efforts to clear the road blockage and continue to the end of the stage. The first competitor to pass through shall notify event officials of the situation via the finish control.

In the case of 2 and 3 above, scoring shall be as per NRR 15.13.

15.11 Maximum Average Speed for Stages

- 15.11.1 While ideally the average speed of the fastest competitor should be lower than 120 km/h (see NRR 13.2.1 and 13.8), any competitor who, in a stage not run on a circuit designed for automobile racing, exceeds an average speed of 120.0 km/h shall be scored with the time taken.
- 15.11.2 In addition to 15.11.1 above, any stage in which any competitor exceeds an average speed of 120.0 km/h. plus 10% shall not count in the scoring of the event except if the stage is run on a circuit designed for automobile racing.
- 15.11.3 The distance of the stage to be used for calculation of the average speed shall be from the start line of the stage to the flying finish of the stage.

15.12 Special Stages

- 15.12.1 Special stages are speed tests on roads closed for the rally. For special stages timing will be completed to the second. However, timing to the tenth of a second may be used for national rallies when electronic equipment is used and notice has been given to competitors.

- 15.12.2 Crews are forbidden to drive in the opposite direction to that of the special stage, under pain of exclusion by the organizer.
- 15.12.3 A special stage will commence from a standing start, with the car placed on the starting line.
- 15.12.4 Starts of special stages will be given as follows:
- 15.12.4.1 When the car with its crew on board has stopped in front of the starting control, the marshal will enter the time scheduled for the start of the car in question on special stage time card (hour and minute). He will hand this document back to the crew and will count down aloud: 30 - 15 - 10 and the last five seconds one by one.
- Alternatively, this method may be replaced by an electronic countdown system, counting down by seconds and clearly visible to the crew from the start position. This system may be electronically coupled to a start line detection device that records any situation where a car leaves the start line ahead of the correct signal.
- 15.12.4.2 When the last five seconds have elapsed, the starting signal shall be given whereupon the car must start immediately.
- 15.12.5 The start of a special stage may only be delayed in relation to the scheduled starting time by the marshal in a case of force majeure.
- 15.12.6 In the event of a late start ascribable to the crew, the marshal will enter a new time, the penalty then being 1 minute per minute or fraction of a minute late.
- 15.12.7 A false start, particularly one made before the marshal has given the signal, shall be penalised by 2 minutes. This penalty does not exclude heavier penalties being inflicted by the Steward(s), especially if the offence is repeated.
- 15.12.8 Special stages will end in a flying finish, stopping between the yellow warning sign and the stop sign being forbidden on pain of exclusion. Timing will be recorded on the finish line. At the finish of special stages, the time-keepers must be positioned even with the timing-line.
- 15.12.9 The crew must then report to the Stop Point indicated by a red "STOP" sign to have its finishing time entered on the special stage time card (hour, minute, and second or hour, minute, seconds and tenth of a second). If the timekeepers cannot give the exact finishing time to the marshals immediately, the latter will only stamp the crew's special stage time card and the time will be entered at the next neutralisation area or regrouping control.
- 15.12.10 If, through a fault of the crew, the time entry cannot be made, the following penalties shall be imposed:
- | | |
|-----------------------------|------------------------|
| at the start: | exclusion |
| at the finish (Stop Point): | 5-minute time penalty. |
- 15.12.11 The times recorded by the crews in each special stage, expressed in hours, minutes, seconds or hours, minutes, seconds and tenths of a second shall be added to their other penalties (road, technical, etc.) expressed in time.

15.13 Interruption of a Special Stage

- 15.13.1 When a special stage has to be stopped for any reason whatsoever before the last crew has covered it, the organizer may establish a classification for the stage ~~may nevertheless be established~~ by allocating to each crew which has been affected by ~~the circumstances of~~ the interruption, the slowest time set before the stage was stopped or the fastest time set by a car that has subsequently completed the stage after the blockage has been cleared if there was no time set before the stage was stopped. ~~Under exceptional circumstances, another time that is deemed appropriate may be assigned.~~ An organizer, for stage safety considerations or for the purpose of clearing the route, may instruct a crew to transit through any stage or to take an alternate route to rejoin the event. In the case of an alternate route being given, exclusion under NRR 17.6 does not apply.
- 15.13.2 This classification may be drawn up even if only one crew has been able to cover the stage in normal competitive conditions.
- ~~15.13.3 Only the Steward(s) may apply this disposition after receiving from the clerk of the course the reasons for the termination.~~
- ~~15.13.4~~ 15.13.3 Should the organizer ~~Steward(s)~~ consider the slowest time set as abnormal, they may choose as the scratch time the one among the four other slowest which seems the most suitable. Under special circumstances, another time that is deemed appropriate may be assigned.

15.13.4 No competitor responsible in total or in part for the interruption of a Special Stage may benefit from this measure. Provided that crew is able to proceed to the finish of that stage, that crew will be given the time it set for the stage if this is greater than the time awarded to the other competitors affected by the interruption. If the time is less than the time awarded to the other competitors, then that crew will be awarded the same time that was awarded to the other competitors.

15.13.5 In addition to the circumstances of 15.13.1 above, if an organizer finds

- (a) that the competition field has become divided by time;
- (b) the course closing vehicle, comes upon a crew stopped in a transit or stage that would not be able to resume competition momentarily; and
- (c) a crew, while still within maximum lateness, has not left a re-grouping control, a reseed or a service within 5 minutes of the last crew's time out,

the organizer may invoke 15.13.1 to 15.13.4.

15.14 DNF

Competitors who are beyond their maximum lateness, have surrendered their timing card to a rally official, have missed a control or are unable to continue as the registered crew shall not compete further in the rally. Competitors who have not been informed by the organizerSteward(s) that they are beyond maximum lateness may continue in the rally. (See NRR 15.5.6) Competitors failing to comply with this regulation shall be subject to sanction at the discretion of the Steward(s).

16. CONTROLS

16.1 General

16.1.1 No controls, other than observation controls, may be placed in the following locations (meal, gas and service stops excepted):

- (a) On a main thoroughfare (this is intended to include most paved roads and most numbered highways).
- (b) Within a built-up area having a reduced maximum speed.
- (c) Within 100 meters of a habitation unless the occupant has consented.

16.1.2 Controls open at least 15 minutes before the due time of arrival of the first car and close no sooner than 15 minutes after the maximum lateness time of the last car in order to allow for dead time and time outs. A control may be authorised to close at an earlier time by a person in the control-closing car if all cars still competing have checked in.

16.1.3 At all controls where time is recorded or in the instructions, the official distance and due time of car "0" should be available to competitors.

16.1.4 Except where specifically provided for elsewhere in this section (NRR 16), the beginning of the control area is marked by a warning sign on a yellow background. At a distance of no less than 25 m, the position of the control is indicated by an identical sign on a red background. The end of the control area, approximately 25m further on, is indicated by a final sign on a beige background with three black transversal stripes. (See NRR 16.9.1)

16.1.5 All control areas are considered as Parc Fermé and are subject to the provisions of NRR 16.8

16.1.6 The stopping time within any control area must not exceed the time necessary for carrying out control operations.

16.1.7 The target check-in time is the responsibility of the crews alone, who may consult the official time at the control.

16.1.8 The marshals may not give them any information on this target check-in time.

16.1.9 Crews are obliged to follow the instructions of the marshal in charge of any controls. Failure to do so will result in a penalty which may go as far as exclusion, at the discretion of the Steward(s).

16.2 Time Controls

16.2.1 At these controls, the marshals mark on the time card the time at which the card was handed in including where it is physically impossible, because of obstruction, for the vehicle to reach the control. Timing is recorded to the complete minute.

16.2.2 A stage start time at least two minutes after the arrival time should be assigned by the control marshal.

16.3 Passage Controls

Marshals are to stamp and/or sign the time card as soon as this is handed in by the crew, without mentioning the time of passage.

16.4 Start Controls

These controls are located at the start of special stages, about 50 meters after the time control. Each vehicle will start a special stage at the 00 second mark of its departure minute, as assigned at the time control with both drivers' safety harnesses and helmets properly fastened.

16.5 Finish Controls

These controls are located at the end of special stages and time will be recorded as the vehicle passes the control board (flying finish). At a suitable stopping distance after the flying finish (about 200 meters), there will be a control where vehicles must stop to receive the timing sticker.

16.6 Main Time Controls

These controls can be located at the start and finish of each section of the rally and at Parc Fermé, but not at the start or exit of a service area. Early arrival is permitted and drivers may check in early but must request their correct time. Drivers shall check out of these controls on their correct minute.

16.7 Observation Controls

16.7.1 These controls will NOT be identified in the route book. They may be located:

- 16.7.1.1 Within or at the end of quiet zones for the purpose of verifying that vehicles comply with the following quiet zone regulations:
- (a) Posted speed limits must be observed.
 - (b) Auxiliary headlights may not be used.
 - (c) Excessive noise is not permitted.
- 16.7.1.2 Anywhere along the rally route for the purpose of inspecting competing vehicles for the presence of pace notes.
- 16.7.1.3 Anywhere along the rally route for the purpose of inspecting safety equipment and vehicle eligibility.
- 16.7.1.4 Anywhere within a transit for the purposes of informing competitors of any violation of transit speed limits.
- 16.7.2 Time will be recorded as the vehicle passes the control board unless the vehicle is observed to have stopped within sight of the observation control, in which case the time at which the vehicle stopped will be recorded as the arrival time. The correct time of arrival at the control following an observation control shall be calculated as if the observation control does not exist.

16.8 Parc Fermé / Parc Exposé

- 16.8.1 The supplementary regulations and/or route book shall specify the place and time of any Parc Fermé. During the time of operation, surveillance is to be carried out by rally officials; these officials are responsible for its operation.
- 16.8.2 The supplementary regulations and/or the route book shall specify the place and time of any Parc Exposé as well as the conditions for its operation. Unless restricted by these conditions drivers and co-drivers are allowed to remain with their vehicles and competitors and service crews are allowed to work on their vehicles. A Parc Exposé may be used before the start and possibly the restarts and, these cases, the penalty (exclusively pecuniary) for late arrival in the start area shall be \$50.
- 16.8.3 As soon as the drivers have parked their car inside Parc Fermé, they must leave the Parc Fermé. They will be allowed to re-enter (or enter) the Parc Fermé 10 minutes before their re-start (or start) time. The target starting time will appear on each crew's time card or route book. Any violations will result in a time penalty similar to early/late arrival at a time control. (Organisers may alter, for publicity purposes, requirements of having crews leave Parc Fermé. However, alterations must be explained in the supplementary regulations and/or route book.) If a regrouping park does not exceed 20 minutes, crews may remain in this regrouping park.
- 16.8.4 Except where specifically provided for elsewhere in the regulations, while the vehicles are subject to Parc Fermé rules, any repair, assistance or refuelling is strictly forbidden, under pain of exclusion. The cars shall be subject to the Parc Fermé rules:
- 16.8.4.1 from the moment they enter a starting area (if one exists) or a regrouping area, until they leave it;
 - 16.8.4.2 from the moment they enter a control area until they leave it;
 - 16.8.4.3 from the moment they enter a refueling zone until they leave it;
 - 16.8.4.4 from the moment they reach the end of the rally until the time for lodging an inquiry as per GCR. 8.2(c) has expired and the Steward(s) have authorised the opening of the Parc Fermé.
- 16.8.5 If the organiser, scrutineer or the Steward(s) consider that the state of a car has become so defective that the safety of normal road traffic might be affected, this car must be repaired in the presence of an official. The time needed for the repair must be considered as so many minutes late, so as to prevent a crew from trying to regain the time lost during such repairs, the penalty for which is 1 minute or fraction of a minute. The crew will be given a new starting time after the repair if the time taken results in any delay beyond the originally scheduled start time.
- 16.8.6 By way of exception, and under the supervision of an authorised marshal or scrutineer, the crew may, while in the Parc Fermé, at the start, regrouping area or end of leg, change the windscreen or rear window with the possibility of outside help. If, in order to change the windscreen, it is necessary to straighten the bodywork and/or safety cage, the provisions described in 16.8.5 above will apply.
- These repairs must be completed before the starting time, otherwise a penalty shall be imposed in accordance with the provisions set out in 16.8.5 above.
- 16.8.7 Any late arrival, ascribable to the crew, at the start of the rally or of a leg, or section, shall be penalised by 10 seconds for every minute or fraction of a minute late. Any crew reporting more than 15 minutes late shall not be allowed to start.

Since the crews have 15 minutes within which to report at the start of the rally, of a leg or of a section, if they report within these 15 minutes the exact starting time shall be entered on the time card. The prescribed minimum interval between the cars must be respected.

- 16.8.8 Only the officials on duty and/or the members of the crew are authorised to push a competing car inside or at the entrance to or exit from a Parc Fermé.
- 16.8.9 Inside the Parc Fermé, the engines may be started by means of an external battery. This battery must not then be taken aboard the car.

16.9 Control Markers

All controls, i.e. time controls, passage and observation controls, starting and finishing controls of special stages and stop points, are indicated by means of standardised signs complying with the drawings below.



- 16.9.1 The signage of the control area is carried out by means of three signs. The beginning of the control area is marked by a warning sign on a yellow background. At a distance of no less than 25 m, the position of the control is indicated by an identical sign on a red background. The end of the control area, approximately 25 m further on, is indicated by a final sign on a beige background with three black transversal stripes. (See NRR 15.8.1)

The signs on a yellow background can be made more conspicuous by the addition of a yellow flag, and the signs on a red background, by a red flag.

- 16.9.2 At Time Controls: The beginning of the control area is indicated by sign #1 on a board with a yellow background. The location of the control itself is indicated by sign #1 on a red background, the end of the control area being indicated by sign #5 (beige background).
- 16.9.3 At Passage and Observation Controls: Signs will be displayed in the same manner as at time controls, except that the sign used will be #2 at the beginning of the control area and at the control, with sign #5 to indicate the end of the control area.
- 16.9.4 At Special Stages: The starting point is indicated by means of a sign #3 on a red background. The finish is preceded by sign #4 on a yellow background. The finishing line, where times will be taken, is indicated by sign #4 on a red background. A "STOP" sign (sign #6) on a red background is placed approximately 100 to 300m further on. The yellow and red signs at the finish control shall be placed on both sides of the road.
- 16.9.5 Refuelling Zone Signs: The refuelling zone signs show a black fuel pump on a blue background within a black circle (sign #7). The end of the refuelling zone sign is the same as the start of zone sign but with three black transversal stripes. Additional red warning signs prohibiting smoking, the use of cellular telephones, naked flame, etc. are recommended.
- 16.9.6 Radio communication points are indicated with a yellow sign #8, approximately 50m before the radio position, and a blue sign #8 at the radio position. If the radio is within a control zone, the yellow advance sign is omitted.
- 16.9.7 The use of control markers described in this section will be mandatory for all rallies.

16.10 Missing Control

Should a control not be placed, the correct time of arrival at the next control shall be calculated using the elapsed times and "lateness" times as listed in the route book.

17. ASSESSMENT OF PERFORMANCE

17.1 Start

Each entry shall start the event with zero penalties and the entry with the lowest penalty (in minutes and seconds or minutes and hundredths of minutes) at the finish shall be declared the winner.

17.2 Penalties

Penalties shall be assessed by the organizers as follows:

On special stages	- Time taken
Each minute late at a time control or main time control	- 10 seconds
Each minute early at a time control or main time control	- 1 Minute
Making a false start to a special stage	- 2 Minutes per offence
Speeding in a service park	- 2 Minutes per offence
Shortcutting a stage	- stage lateness time
Striking a chicane	- 15 seconds per element struck

Exceeding speed limit at Observation Controls

1-10 km/h over the limit = 0 seconds

11 km/h = 10s	16 km/h = 65s	21 km/h = 145s	26 km/h = 250s
12 km/h = 20s	17 km/h = 80s	22 km/h = 165s	27 km/h = 275s
13 km/h = 30s	18 km/h = 95s	23 km/h = 185s	28 km/h = 300s
14 km/h = 40s	19 km/h = 110s	24 km/h = 205s	29 km/h = 325s
15 km/h = 50s	20 km/h = 125s	25 km/h = 225s	30 km/h = 350s

Over 30 km/h over the limit = disqualification

No time penalty shall be assessed for infractions of a non-competitive or administrative nature.

17.3 Improper Procedure Infractions

Improper Procedure Infractions must be reported in writing to the organiser by marshals, judges of fact or competitors. Upon confirmation of the infraction or when a judgement by a judge of fact a penalty of 10 minutes will be applied for a first infraction and may result in a recommendation by the organizer to the steward(s) for a sentence of exclusion for a second violation during the event. Use of in-car video footage may be used by event or CARS officials to verify compliance with the regulations for use of safety warning devices (see GCR 3.11) For 17.3.4 below, in addition to the 10 minute penalty applied to the crew, a \$500 fine will be assessed to the entrant by the steward(s) of the event.

- 17.3.1 Serious violation of quiet zone regulations.
- 17.3.2 Failure to properly wear safety harness, driving suit, helmet, or head and neck restraint while traversing a special stage.
- 17.3.3 Reversing in a control area.
- 17.3.4 Violation of procedures detailed in NRR 17.9.
- 17.3.5 Traversing a stage with the sun roof open or roof panels removed.
- 17.3.6 Traversing a stage with window(s) open (more than 2.5 cm) and without window net(s) in place.
- 17.3.7 Illegal service.

17.4 Timing Errors

The organiser reserves the right, when scoring any time card, to correct whole-minute timing errors.

17.5 Baulking

No claim is allowable for baulking. However, the Steward(s) may exclude an entry found to have unreasonably delayed another entry.

17.6 Finishing Qualifications

To qualify as a finisher, an entry shall check in at every control in the rally within the MPE-MPL.

- 17.6.1 A competing vehicle shall complete the entire course under its own power. (Failure to do so will result in exclusion.) Exceptions are listed below.

- (a) By a ferry, the use of which is required by the route of the event.

- (b) By outside means for the minimum distance necessary to extricate it from difficulty, or to clear the route for other competitors.
- (c) By gravity.
- (d) By the unaided efforts of its crew (including pushing out of a Parc Fermé).
- (e) By official permission or instruction.

In the case of (b), (c) or (d) above, any crew found to have delayed another crew by their action shall be excluded from the event.

17.7 Serious Violation

The organiser may request the Steward(s) to assess a fine, not exceeding the competitors' entry fee, or recommend exclusion by the Steward(s), to either or both of the crew members for any action not specified above, and in particular for being charged for a violation of any traffic regulation, for breach of regulations governing the behaviour of service crews or unsportsmanlike conduct.

17.8 Practicing/Pace Notes

17.8.1 Except where there is a reconnaissance opportunity or a notes familiarization pass opportunity (ies) is provided, practicing in the area of any event shall be banned for a period of 60 days prior to each rally. Practicing shall be deemed to include a wide range of actions including but not limited to: testing a competition vehicle, production or clarification of notes for a stage, actions which annoy residents. It is the intent of this rule that competitors, without legitimate business, shall not drive on stage roads. (Permission may be granted by an organiser to traverse specific portions of a route for familiarisation purposes, but it must be granted to all competitors.)

17.8.2 Except in reconnaissance situations, possession of pace notes made prior to the event is prohibited. Pace notes are defined as any form of additional descriptive notes or any other information depicting the rally route which is not provided by the route book, organizer-supplied notes or course notes. (see NRR 14.4 and NRR 14.5)

17.8.3 Penalties for violation of 17.8.1 and/or 17.8.2 above are those detailed in GCR 7.4.

17.8.4 Competitors are reminded of the reciprocal agreements with the RA and NASA regarding violations of 1 and 2 above. Canadian licences will be suspended for competitors practising in the USA and vice versa. RA and NASA sanctions for competitors practicing in the USA will be enforced by CARS and vice versa.

17.9 Use of Safety Warning Devices

17.9.1 In cases where a vehicle is stopped:

17.9.1.1 ONE warning triangle must be placed at the same side of the road as the stopped vehicle, facing oncoming rally traffic, a sufficient distance from the stopped vehicle to give ample warning to oncoming competitors.

17.9.1.2 If the vehicle is unable to continue the rally, ONE triangle must also be placed ON the vehicle.

17.9.1.3 Competitors arriving at a location where a triangle is displayed shall SLOW to a reasonable speed and maintain that speed until they have passed the stopped vehicle (no scoring relief will be given). Competitors are reminded that one triangle could indicate a complete route blockage.

17.9.2 In the event of injuries, competitors are expected to display the **fluorescent-red** cross symbol (back cover of the route book) and following competitors are required to stop and render aid.

17.9.3 If no triangle is displayed, competitors arriving at a location where another competing vehicle has stopped, must stop until the safety of the occupants of the stopped vehicle has been ascertained and the set-up of the stopped vehicle's triangle has started. Vehicles stopping to comply with this rule may apply for scoring relief under 15.13.

17.9.4 If a crew is stopped in a stage, unable to continue, but do not need medical attention, they must display the "OK" sign found on the reverse of the red cross, to following rally cars.

17.9.5 The organiser, upon inquiry and only with the concurrence of the Steward(s) of the meeting, shall have the right to grant a time allowance to a competitor who has stopped to ascertain the need for assistance as per section 2 above.

17.9.6 Display of a Red Cross symbol on a stage will be cause for possible cancellation of the stage by the organiser with the concurrence of the stewards.

17.9.7 Self-igniting flares may not be used at any time on stages.

17.9.8 Competitors having displayed any triangles must recover them before continuing any further in the rally.

17.9.9 Penalties for violation of the above shall be according to NRR 17.3.

17.10 Slow Moving Vehicles

The use of 4-way flashers is recommended for competitors who are able to proceed through the stage but are unable to maintain a reasonable speed.

18. SERVICE CREWS

18.1 General

- 18.1.1 A service crew shall consist of registered vehicles and their occupants.
- 18.1.2 Each service crew member must be registered and sign a waiver before the event.
- 18.1.3 Only one service crew may be registered by any one competing crew. However, any registered service crew may service any competing vehicle.
- 18.1.4 Any misdemeanour or infraction of the regulations by a service crew may result in penalties being assessed against their registered competing crews.
- 18.1.5 The organisers should provide:
 - (a) Instructions for service crews, giving locations of approved service areas and the expected arrival time and direction of car "0". Instructions for service crews should be available at least three hours before departure of the first vehicle.
 - (b) Markings for service vehicles.
 - (c) Additional information as required to ensure safe operation of the service areas.
- 18.1.6 Service areas will be considered quiet zones and residents adjacent to such areas must be made aware of their operation.
- 18.1.7 Service is defined as work carried out on the car by any person other than the competing crew, or the use or receipt by the crew of any manufactured materials (solid or liquid), spare parts, tools or equipment other than those carried in the competing car. Such materials carried in the competing vehicle, while on route, may not be left behind at any location to be used later during the rally. For competing cars waiting at the time control at the entrance to regroup or service parks or parked inside regroup, the passing of food, drink and information (data, road book, etc.) to or from the crew is permitted. Competitors may, while on route (and in non Parc Fermé conditions), offer both assistance and materials from their vehicle to other competitors seeking assistance. However, such gestures must be in a sporting nature and not planned in advance or contrived. Violations of the above will be considered illegal service (see 17.3.7).
- 18.1.8 All service shall be confined to official service parks.
- 18.1.9 All Service Crews must have at least one fire extinguisher with a minimum UL rating of 40BC or two fire extinguishers with a minimum UL rating of 20BC each in their respective service area. The fire extinguisher shall be easily accessible and in an area visible to anyone within the teams' respective service spot.

Where a fire service is not present, service crews must carry at least one fire extinguisher with a minimum UL rating of 10BC, other than the fire extinguisher on board the competition vehicle, to all refuelling zones and remote/optional refuelling zones.

If requested by an official, evidence must be produced that fire extinguishers have been purchased or recharged within the preceding two years.

19. CONTENTS OF SUPPLEMENTARY REGULATIONS

19.1 The Organiser

The organiser must provide a set of supplementary regulations specific to the rally. These regulations may complement but must not contradict the GCRs or the CARS Rally Regulations unless approval for specific exemptions has been given as per GCR 1.9.

19.2 As a Minimum

The supplementary regulations must include the following:

19.2.1 A statement of jurisdiction, including the wording "The rally will be held under the Rally General Competition Rules of the Canadian Association of Rallysport, the current CARS Rally Regulations and the rally regulations of _____ Region. These publications are available from _____ (e.g.: CARS Director of the host region, region office or other mailing address and telephone number) and will be available for scrutiny at the start."

19.2.2 The name of the rally.

19.2.3 The name of the organising club and the name, address and telephone number of the following:

- (a) The organiser.
- (b) The registrar (to whom entries are to be sent).
- (c) The publicity chairperson (to whom team promotional materials are to be sent).
- (d) The Clerk of the Course

The names of the following should be included if available:

- (a) The chief scrutineer.
- (b) The chief marshal.
- (c) The chief scorer.

19.2.4 The status of the rally, indicating any championships of which it is a part.

19.2.5 The locations of registration, technical inspection, the start, and the finish with a map or directions from the nearest major highway(s).

19.2.6 A schedule of dates and times giving at least:

- (a) Opening and closing of registration and technical inspection.
- (b) Introduction of officials.
- (c) Departure time of the first vehicle.
- (d) Approximate due time of arrival of the first vehicle at the finish.

19.2.7 A brief description of the rally indicating:

- (a) Approximate total distance.
- (b) Approximate percentage of special stage distance.
- (c) Outline the type of event it will be (see NRR 14.1.1) and provide the details and schedules needed including rules for reconnaissance or a notes familiarization pass (if used).

19.2.8 The minimum and maximum altitudes for the stages of the rally.

19.2.9 The closing date for entries.

19.2.10 The maximum number of entries to be accepted and how entries will be chosen if more than this number are received.

19.2.11 The amount of individual and team entry fees (including CARS levies). If the fees include the cost of any food, fuel, accommodations, etc., it must be so stated.

19.2.12 Optionally, a list of all awards, detailing for what placing each award will be given. This must be posted at the opening of registration if not in the supplementary regulations.

19.2.13 The following documents:

- (a) An entry form.
- (b) A copy of the official CARS waiver form for rallies.

19.2.14 A statement of any conditions of eligibility of vehicle, crew and equipment in addition to those described in NRR 11 and 12 above.

- 19.2.15 Any region rally regulations which require unusual conditions to be met by the competitor in advance of the rally.
- 19.2.16 The maximum distance between gas stops, if greater than 150 kilometres. (See NRR 13.2.5)
- 19.2.17 Any other regulations which the organisers, promoters and/or the region wish to apply to the rally.
- 19.2.18 If an event provides organizer-supplied notes for the special stages, this must be stated in the supplementary regulations, and the format of those notes must be described, as well as any additional fees.

19.3 Alterations

Should it be necessary to alter these regulations, the approval of the CARS President and the appropriate CARS Director of the host region must be obtained and the alteration must be printed in the supplementary regulations and posted on the official notice board.

20. RESULTS

20.1 Posting Requirements

- 20.1.1 Complete provisional scores (control-by-control) are to be posted at one time for all competitors. They should be posted not less than 30 minutes and not more than 90 minutes after the arrival of the last possible finisher.
- 20.1.2 Failure to post complete scores before the latter time means that provisional results are to be mailed. Complete scores may be posted at a later time for public relations purposes and general information of competitors, but they should be considered unofficial. Grievances may not be entered concerning unofficial results.
- 20.1.3 Leg totals for any or all competitors may be posted at any time for public relations purposes and general information of competitors. Partial scores so posted should be considered unofficial. Grievances may not be entered concerning unofficial scores.
- 20.1.4 The score board must show exactly the penalties calculated from the route card of each competitor.

20.2 Posting Time

The posting time of scores shall be noted on the score board by the senior steward at the time the results are posted.

20.3 Revisions to Results

Corrections to scores as a result of grievances should be made as soon as possible and are to be posted at the finish of the rally. The time of modifications should be noted adjacent to each total score modified.

20.4 Finalisation of Results

When the grievance proceedings have finished (see NRR 23.2), the results are final if all the following conditions have been met:

- (a) The scores were posted within the time limits allowed.
- (b) Any questions concerning proper class classification of any vehicle have been dealt with.
- (c) No protest decisions have been deferred as in GCRs 9.5(e)(2).
- (d) No appeals have been submitted.

20.5 Distribution of Results

- 20.5.1 Results must be sent by the organiser by first class mail, e-mail or web-posting to all competitors and entrants within fifteen (15) days of the finish of the rally.
- 20.5.2 If the rally is a part of a regional championship, results must be sent by first class mail or by e-mail and web-posting to the Steward(s), the CARS Director of the host region, the region championship scorer (if one has been appointed) and the national office.
- 20.5.3 If the rally is a part of the Canadian championship, results are to be sent by first class mail or by e-mail to the Steward(s), all directors of CARS, the CARS President, the National Series Manager, the scorer and the CARS office.

20.6 Publication of Results Subsequent to a Protest

If a protest meeting is held as described under GCRs 9.5 and if protests are considered, another set of provisional results are to be issued containing:

- (a) The Steward's(s) decision on all protests considered.
- (b) A complete set of scores (control-by-control) modified according to the decisions, if scores are modified.

These sets of provisional results are to be sent by first class mail to all persons described in NRR 20.5 within fifteen (15) days of the protest meeting.

20.7 Delay by Appeal

- 20.7.1 If an appeal meeting is held as described under GCRs 10 and if results are modified, a set of results are to be issued containing:
 - (a) The appeal board's decisions on all appeals heard.
 - (b) A complete set of scores (control-by-control) modified according to the decision(s).
- 20.7.2 This set of results should be declared final and are to be sent by first class mail to all persons described in NRR 20.5 within fifteen (15) days of the appeal meeting.

20.8 Provisional Results

Previously issued provisional results may be declared final without change if:

- (a) A protest meeting was held as described under GCRs 9.5 and no protests were considered.
- (b) An appeal meeting was held and the results were not modified by the decisions of the appeal board.
- (c) The time limit of submission of appeals (see GCRs 10.3) has expired and no appeals are to be heard.

In this instance the organiser should send by first class mail to the persons described in NRR 20.5 a notice stating that the provisional results as previously mailed are final as of a specific date and containing (in the case of (a) or (b) above) the decisions of the protest or appeal board. Such a notice should be mailed within fifteen (15) days of the date when results were declared final.

20.9 Distributed Results

Distributed results must contain the following information:

- (a) Name of the rally.
- (b) Name of organising club.
- (c) Date of rally.
- (d) Status of rally.
- (e) Permit number.
- (f) Exact official mileage of rally, including transits and stages as well as the length of each stage.
- (g) Finishing positions listed in order (overall, driver and vehicle classes), including total times.
- (h) Marque, model and class of all entered vehicles.
- (i) Vehicle number.
- (j) Full names of both crew members, event(s) entered and driver class.
- (k) Time taken for each stage as well as time penalties incurred at any control.
- (l) Any other penalties assessed against the crew.
- (m) Complete list of prize winners, including any purse or tow money awarded.
- (n) A statement as to the status of the results, i.e.:
 - Final.
 - Provisional due to scores not being posted according to NRR 20.1.
 - Provisional, containing a deferred protest decision.
 - Provisional, containing the decision(s) of a protest meeting held in accordance with GCRs 9.5 and subject only to appeal.
 - Provisional due to an appeal or pending appeal.
 - Provisional, containing scores modified as a result of the decision of an appeal board.

20.10 Ties

20.10.1 Competitors having equal numbers of penalty points shall be considered tied. Subsequent positions shall be enumerated on the basis of the number of vehicles ahead.

20.10.2 In Canadian championship rallies, equal positions and championship points are to be awarded. A tie-breaker may be applied to the distribution of awards if specified in the supplementary regulations.

20.11 Tests

Driving skills tests, gymkhanas, etc., which may be held in conjunction with championship rallies, should not be scored as part of the rally event for championship points.

20.12 Revisions

The Steward(s) of the event should declare amendment(s) to the results as required to reflect decisions regarding the reclassification of competitors as a result of grievances.

21. ADMINISTRATION OF A RALLY

21.1 Route Check

21.1.1 Organizers are responsible for appointing a green crew to see that the route is checked prior to the rally from final instructions, at the time of day and under the same conditions as anticipated for the event itself. The purposes of this check are:

- (a) To ensure that the route and instructions comply with CARS Rally Regulations and the supplementary regulations.
- (b) To check the suitability of the start, finish, gas, meal and rest stop facilities.
- (c) To find any ambiguous instructions, incorrect instructions, distance errors, etc.
- (d) To check the location of controls to ensure that they are safe and in compliance with all applicable regulations.
- (e) To check the timing calculations to verify they are correct.
- (f) To check the time allowances and ensure that they are safe and within the regulations, paying particular attention to built-up areas and busy main roads.

21.1.2 The green crew should advise the organiser in writing of any changes required. A copy of the green crew's report to the organiser is to be sent to the Steward(s) as well.

21.2 Starting Order

21.2.1 CARS shall maintain a driver's seed list in which all drivers will be placed in the highest seed for which they qualify and organizers should use this seed list to help determine the event start order. Foreign drivers should be included in the seed that best reflects their qualifications.

21.2.2 Based on the criteria in NRR 21.2.14 below, a seeded draw is to take place at the time and place provided for in the event supplementary regulations prior to each event to determine the start order as well as the vehicle number. This seeded draw shall be for: Seed 1 (Group 2 and 5, Production Sport), Seed 2 through Seed 6 drivers. The organizers of a rally have the right, after approval by the Senior Steward, to place any driver in a specific position in the Start Order to reflect their demonstrated speed in previous stage events.

21.2.3 For seed one drivers (Open and Production GT), a meeting at the event is to take place at the time and place provided for in the event supplementary regulations to determine the start order. (Each seed one driver entered will be assigned a vehicle number based on the CRC standings as of the start of the event or, in the case of the first event of the year, the final standings of the previous championship calendar with any ties in the championship standings being broken by the organizers by a coin toss.) The starting positions available shall equal the number of seed one drivers entered in the event. The procedure for determining the start order will be as follows:

- (a) A draw will be conducted whereby those seed 1 drivers (not co-driver) present at the time of the draw shall each draw a number;
- (b) The order of the draw will be by the drivers' current championship standings, or in the case the first event of the season, the championship standings for the previous season;
- (c) The number drawn will be for the order of selection of the driver's start position (The choice of numbers will equal the number of seed one drivers present at the time of the draw.);
- (d) The seed one driver who has drawn the first choice shall be the first to select a start position from the start positions available and the seed one driver who has the second choice shall then select a start position from the remaining available positions, etc.;
- (e) For any seed one driver not present at the draw, the organizers shall assign a starting position from the remaining positions available by draw. The seed 1 driver's draw for starting positions will not change the vehicle number assigned by the event organizers.

21.2.4 There should be seven seed groups. The qualifications for each of these groups are as follows:

Seed Qualifications:

- Seed 1: All drivers with 80 or more seed points as a driver.
- Seed 2: All drivers with a minimum of 25 and less than 80 seeding points as a driver.
- Seed 3: All drivers with a minimum of 10 and less than 25 seeding points as a driver.
- Seed 4: All drivers with a minimum of 1 and less than 10 seeding points as a driver.
- Seed 5A: All drivers having finished eleventh through fifteenth of a Canadian Rally Championship event.
- Seed 5B: All drivers having finished one performance event but not qualifying for seed 5A as a driver.
- Seed 6: All others.

21.2.5 Events that qualify for seeding points are recognised National Championship events. The onus is on the competitor to provide documentation of results outside of Canada to CARS for consideration.

- 21.2.6 When calculating seeding points, the best two of the preceding three year's total points are counted plus the points of the current year. This permits drivers to improve their seed groups within the current year. Drivers who earn a higher seed shall retain that seed for the following competition year unless they further improve their seeding.
- 21.2.7 Points should be awarded to the top ten (10) finishing positions in each of the qualifying events per the Canadian Rally Championship points schedule (see NRR 25.1.6), not including start/finish points.
- 21.2.8 In NO case may a competitor lose more than one seed group from the highest seed group which they have ever attained beginning with the 89/01/01 seed list. Drivers with Master or Grand Master status will not drop a seed group, until the fifth year after they cease to be active. Any seeded driver who does not finish an event for which seeding points are awarded over a two year period will be dropped from the seeded drivers' list. Re-entry onto the list will be on a case by case basis.
- 21.2.9 Entries received after the draw, but before the opening of registration, are to be started at the end of their proper seed group. The organiser's decision on starting order is final as long as it complies with these regulations.
- 21.2.10 A seeding list will be provided to organisers prior to the seeded draw for each event. This list will be maintained in an updated condition by the Canadian Rally Championship scorer or his/her delegate. It may occur that a driver's qualifications for a higher seed group is at an event subsequent to the most recent update of the official seed list. If this situation occurs, the first driver shall be responsible for proving his improved seed position or will be seeded according to the latest list.
- 21.2.11 Competitors are responsible for stating their true seed qualifications upon entry in any event where requested. Providing false information may result in disciplinary action.
- 21.2.12 First drivers improving their seed position after the draw but before the close of registration may, at the discretion of the organiser, be started at the end of the higher seed group for which they have qualified.
- 21.2.13 For the purpose of making seed group awards where there are too few competitors in a seed group, groups may be combined in a forward motion only (i.e.: combining seed groups 2 and 3 would result in seed group 2 awards being made to that combined group). Such combination of seed groups may be performed for award purposes only.
- 21.2.14 The starting order will be:
- | | |
|---------|--------------------------------------|
| Seed 1 | Open 4WD and Production 4WD |
| Seed 1 | Open 2WD and Group 5 |
| Seed 1 | Production 2WD |
| Seed 2 | Open 4WD and Production 4WD |
| Seed 2 | Open 2WD and Group 5 |
| Seed 2 | Production 2WD |
| Seed 3 | Open 4WD and Production 4WD |
| Seed 3 | Open 2WD and Group 5 |
| Seed 3 | Production 2WD |
| Seed 4 | Open 4WD and Production 4WD |
| Seed 4 | Open 2WD and Group 5 |
| Seed 4 | Production 2WD |
| Seed 5A | Open 4WD and Production 4WD |
| Seed 5A | Open 2WD, Group 5 and Production 2WD |
| Seed 5B | Open 4WD and Production 4WD |
| Seed 5B | Open 2WD, Group 5 and Production 2WD |
| Seed 6 | Open 4WD and Production 4WD |
| Seed 6 | Open 2WD, Group 5 and Production 2WD |
- 21.2.15 It is required that rally organisers hold special briefings for all novice designated drivers (and their co-drivers) in seeds 5 and 6 before the start of the event.

21.3 Route

- 21.3.1 A course-checking vehicle is to cover the route not less than six (6) hours or more than twenty-four (24) hours before the rally. If a portion of the route is not passable to the course-checking vehicle, alternate instructions are to be prepared and posted (see NRR 21.8.4).
- 21.3.2 A course sweep vehicle is to follow the control closing vehicle to effect aid to stranded competitors on stage events. Such a vehicle should be able to extricate vehicles that have been immobilised and to transport crews to the nearest service area or location where further assistance can be arranged.

21.4 Controls

- 21.4.1 A control-opening vehicle is to be used to ensure that controls are correctly located and that control timepieces are set correctly.
- 21.4.2 A control-closing vehicle is to be used to ensure that controls are correctly located, that they do not close too soon and that control timepieces are still functioning properly. The control-closing vehicle should also collect the control record sheets and bring them to the finish.
- 21.4.3 If either or both of these vehicles is unable to complete the route for some unforeseen reason, this should not of itself be reason for exclusion of controls in scoring.

21.5 Registration

- 21.5.1 A registration is to be held at the start of the rally during which a registrar is to check that all competitors possess valid documents as listed in these regulations and have signed all waivers in effect for the event.
- 21.5.2 It is recommended that an odometer check using official rally distance be made available to competitors at or before registration. The odometer check should be at least 10 kilometres in length but need not be part of the route.

21.6 Technical Inspection

- 21.6.1 A technical inspection for all competing vehicles is to be held at the start of the rally. The technical inspection consists of: a self-declaration on the road worthiness of the competing vehicle (as below), a check on compliance with CARS safety requirements and vehicle class eligibility.

For each crew, a signed, self-declaration will be submitted at the technical inspection that declares that the competing vehicle is a) road worthy and b) that the following items are operating properly:

- (a) Running lights;
- (b) Horn;
- (c) Wipers;
- (d) Reverse light;
- (e) Brakes;
- (f) Exhaust.

The scrutineer may choose to verify compliance with any aspect of the self-declaration.

- 21.6.2 If the scrutineer determines that a vehicle is not eligible to compete, or is not eligible for the class declared by the entrant, then the scrutineer, with the approval of the Steward(s) of the event, has the right to refuse that vehicle from entering the event, or from entering in the declared class. Based on the reasons for this decision, the CARS Board may cause a hearing to be held at which time it should be decided whether any or all championship points gained by that vehicle prior to the date of failing to pass the inspection should all be allowed to stand, changed to another class or declared void.
- 21.6.3 A thorough and detailed inspection, including (if necessary) dismantling the vehicle, may be made at the finish of any rally at the discretion of the Steward(s) and/or a member of the CARS Board or in the case of competitor inquiry or protest. In the event that dismantling is required, the procedure should be as follows:
- 21.6.3.1 If the inspection, including (if necessary) dismantling the vehicle, is as a result of a competitor inquiry or protest the Senior Steward should determine and collect a deposit from the inquirer/protestor in cash. Should the inquiry/protest be denied, the protestor shall lose the costs involved, even to paying more than the deposited amount. Should the protest be upheld, the inquirer/protestor shall have the deposit refunded, and the vehicle crew found in violation shall pay any and all costs in cash to the Senior Steward who shall remit such costs to the vendor of service and/or supply. Vehicles in violation shall be impounded until satisfaction has been assured in either case above.
- 21.6.3.2 If the inspection, including (if necessary) dismantling the vehicle, is as a result of the decision of the Steward(s) and/or a member of the CARS Board, then the competitor is responsible for any and all costs. Any decision resulting from such inspection/teardown should be subject to appeal only.
- 21.6.3.3 Those persons permitted to attend such a detailed technical inspection are:
- (a) The crew of the car being scrutinised.
 - (b) The mechanic(s) who perform dismantling.
 - (c) The organiser or his/her representative, such as the scrutineer.
 - (d) CARS officials, such as the Steward(s) of the meeting or a member of the CARS Board.
 - (e) A representative of the protestor.

21.7 Introduction of Officials / Drivers' Meeting

An introduction of officials is to be held at least 30 minutes before the start of the rally. The organisers, the Steward(s) and any other persons authorised to sign official documents are to be introduced. No information shall be given that has any pertinence to the running of the route; all such information is to be posted on the official notice board if not included in the route book.

Questions of a general nature or of clarification may be answered during the introduction. All other questions to the organizer should be written and posted on the official notice board along with the response of the organizer.

At least one of the driver or co-driver must attend the Introduction of Officials and any Drivers' Meeting listed in the supplementary regulations or event schedule. Organizers have the right to call an additional driver's meeting in cases of force majeure and with the approval of the Steward(s). Notice for such meetings will be posted on the official notice board and by also be communicated to the entrants by other means.

21.8 Official Notices

- 21.8.1 An official notice board is to be displayed at the start, ends of legs and at the finish and should contain only the permit for the rally, official notices, letters of notification to police and letters of permission to use private roads.
- 21.8.2 All official notices are to be signed by the organiser, a steward or a person designated at the introduction of officials.
- 21.8.3 All types of marker boards to be used at controls, if different from NRR 16.9, are to be displayed near the official notice board at the start.
- 21.8.4 Any known corrections to the route instructions are to be posted on the notice board before the start of the leg to which they apply.
- 21.8.5 If a change to the route instructions becomes necessary after the leg has started, an official notice containing the changes to the route and/or timing is to be posted at a control or on a clearly visible and securely placed control sign. Such a notice is to be placed before the maximum earliness of the first car to pass that point. If such a notice is posted at a control, a marshal should point it out to each competitor who checks in and should obtain verifying signatures by car number. If such a notice is posted on an unmanned control sign, at least two control signs are to be used, one on each side of the road or both on the right side, at least 30 meters apart. An adequate time allowance is to be given.
- 21.8.6 Any changes to the supplementary regulations is to be posted on the official notice board before the start of the leg to which they apply and is to be signed by the organiser and a steward.
- 21.8.7 Factors defined by the organisers as a result of written questions from the competitors are to be posted on the official notice board as soon as possible.
- 21.8.8 Copies of grievances and replies to grievances should be posted adjacent to the official notice board.
- 21.8.9 An official with the authority to sign official notices is to be at the end of each leg before the due time of arrival of the first car and until the time of departure of the first car on the next leg.
- 21.8.10 Organisers are to post on the official notice board at the conclusion of registration, a complete entry list of competitors stating at least the following; car number, names of driver and co-driver, car, driver classes, vehicle classes, licence affiliation and, if the event includes a regional portion, the event(s) entered.

21.9 Scoring

A suitable number of persons not otherwise connected with the rally (unless as registrar, scrutineer or control-opening or course-checking crews) are to be available as scorers. The scorers are to be informed before the start of the event of the contents of the supplementary regulations, the method of scoring to be used and the correct elapsed times between controls. At each point where scoring is to be done, a scoring area separate from the control and the competitors is to be arranged. The scorers are to be present before the due time of arrival of the first car. The scorers are not to leave the finish of the rally until the grievance proceedings have finished (see NRR 23.2).

21.10 Podium Ceremonies

At the conclusion of competition at a national championship event, separate podium ceremonies for the top three finishers in both the Overall 4WD and Overall 2WD will be held. The event may have other podium ceremonies of its choosing. (The participants in the podiums shall be as best judged by the organizers at the end of the event and before the official conclusion of the inquiry and scoring processes.)

22. STEWARDS

Refer to the GCRs (particularly GCRs 6.2) for further information regarding Steward(s).

22.1 Duties

In addition to those duties set out in the GCRs, Steward(s) shall also be responsible for the following:

22.1.1 When the Steward(s) is satisfied that the green crew's report has been addressed by the organizer, he should notify the national committee member so that the permit may be issued. No further changes should be made without the approval of the Steward(s).

22.1.2 At the finish of the rally (and, as applicable, at the start and end of legs), Steward(s) should:

- (a) Make grievance forms available to competitors.
- (b) Accept inquiries which are submitted on the proper forms and within the time limits described in GCRs 8. (Note that enquiries concerning the calculation of a score as detailed in GCRs 8.2(d) may not be accepted unless the posted score was official as defined in NRR 20.1.)
- (c) Note the time the inquiry was received, post one copy and deliver the remaining copies to the organiser.
- (d) Accept organiser's replies to inquiries, and post them or mark inquiries void as described in GCRs 8.4.
- (e) Accept protests which are submitted on the proper forms and within the time limits described in GCRs 9.2(a) and accompanied by the appropriate fee as specified in GCR 9.11.
- (f) Note the time the protest was received, post one copy and bring the remaining copies to the protest meeting.
- (g) Upon receiving a protest, the Steward(s) should first verify that the protest has been properly submitted according to GCRs 9.2, and should then prepare a notice giving the location and time at which the protest meeting will be held, according to the time limits given in GCRs 9.5(b) & (c). This notice should be posted on the official notice board.
- (h) The Steward(s) should conduct the protest meeting and prepare a reply according to GCRs 9.5(e). If the decision on the protest is deferred, the Steward(s) should obtain such further evidence as deemed necessary, prepare the decision and send it to the organiser according to GCRs 9.5(f).

If applicable (see GCRs 9.2(b) & (c)), the Steward(s) should meet to consider protests submitted by mail according to GCRs 9.5(g). They should then prepare their decision and mail it to the organiser according to GCRs 9.5(h).

- (i) Accept appeals which are submitted according to the GCRs.
- (j) Note the time the appeal was received, return one copy to the appellant and post the remaining copies. Within 24 hours after the finish of the rally, the Steward(s) should send copies of the appeal by first class mail to all CARS Board members.
- (k) After the grievance proceedings have finished (see NRR 23.2), the Steward(s) should obtain one copy of each grievance, reply and notice to be included in the Steward's(s') report. He should retain the remaining copies in case they are needed as evidence at a later appeal meeting.
- (l) At the conclusion of scrutineering, collect all log books and make any post-scrutineering notes as may be required in them.

22.1.3 After the rally, the Steward(s) should receive protests by mail according to GCRs 9.2(b) & (c).

23. GRIEVANCE PROCEDURES

23.1 Grievances

- 23.1.1 A grievance may consist of an inquiry, a protest or an appeal and is subject to the rules set out below and in the GCRs.
- 23.1.2 All grievances submitted at the start, finish or end of a leg of a rally shall be submitted to a steward on grievance forms obtained from him.
- 23.1.3 The regulations contained in this section (NRR 23) shall be considered an extension of those already contained in the GCRs. Additional information can be found in the GCRs.

23.2 The Conclusion of Grievance Procedures

The grievance proceedings at the finish of a rally have concluded when;

- 23.2.1 Each inquiry posted has been marked "void" and 30 minutes have elapsed since the reply was posted or the inquiry marked "void".
- 23.2.2 Each protest has been heard at a protest meeting and the decision has been deferred or the decision has been posted and 30 minutes have elapsed since the decision was posted.
- 23.2.3 All competitors have checked in at the finish and 30 minutes have elapsed since the time-in of the last car or 30 minutes have elapsed since the maximum lateness for the last car to arrive at the finish.
- 23.2.4 The scores have been posted according to NRR 20.1. The modifications resulting from all posted decisions have been made to the scores and 30 minutes have elapsed since any score has been posted or modified.

The organiser, the Stewards and the scorers must remain at the finish of the rally until the grievance proceedings have finished.

24. QUALIFICATION FOR REGIONAL OR NATIONAL

24.1 Applications for Events

24.1.1 Nationally listed events.

- (a) Existing events in the CRC may submit their desire to remain in the championship for the following year via communications with the National Series Manager.
- (b) Desired calendar dates for the following championship year are to be sent by the national event organizers to the National Series Manager by September 1 of the current year.
- (c) Any new event wishing to become part of the Canadian Rally Championship for the following calendar year and who has been observed for national status, may apply through the National Series Manager by September 1 of the current year.
- (d) The National Series Manager shall inform the CARS board which events wish to be part of the following year's championship along with their desired calendar dates in time for the board's September conference call.
- (e) The CARS board will determine the composition of the following year's championship during their September conference call.
- (f) The CARS office will send out application forms and timelines to the event organizers before the end of the current calendar year.
- (g) Event permits will be given to the event organizer by the National Series Manager before the opening of the registration period at the event and upon verification that event insurance is in place and that the route has been checked.

24.1.2 Regionally listed events.

- (a) The CARS Director of the host region should be responsible for distributing permit application forms to the organising clubs in their respective regions.
- (b) The organiser is to complete the form and return it to the CARS Director of the host region together with a cheque for the permit fee (dated the date of application) before the regional fall meeting.
- (c) The CARS Director of the host region should issue the permit when NRR 24.5 and 24.6 of these regulations have been complied with.

24.1.3 The organiser is to apply for insurance through the CARS office. The permit is not to be issued until insurance requirements have been met.

24.2 Local Requirements

A rally applying for regional status is to comply with any further regulations specified by the region concerned.

24.3 Qualifications

24.3.1 A rally must have been run successfully as a regional stage event in order to be eligible for upgrading to National status. A club wishing upgrading of such an event for future national status is to request that the rally be observed for upgrading from the CARS Director of the host region at least 60 days prior to the event. At that time, the CARS Director of the host region is to inform the CARS President, who is to appoint an observer to the event. The CARS President may delegate the appointment of the observer to the CARS Director of the host region.

24.3.2 For a rally to receive national status, the proposed organiser is to have organised a successful regional status event and/or be accepted by the CARS Director of the host region.

24.3.3 A Canadian championship rally should contain a minimum of 150 kilometres of stage distance.

24.3.4 To qualify for international status, a rally is to have been run successfully as a national event.

24.3.5 If the listing for an existing Canadian championship event is dropped, the organisers should be required to reapply for listing as a new event. See NRR 24.3.1 above.

24.3.6 Expenses for the observation shall be the responsibility of the host region and the organizing club.

24.4 Exceptions

Notwithstanding the provisions of NRR 24.3, the CARS Board has the authority to grant national status to any rally under exceptional circumstances.

24.5 Supplementary Regulations

24.5.1 Supplementary regulations for regional championship rallies are to be:

- (a) Submitted for approval to the appropriate CARS Director of the host region at least 60 days prior to the event.
- (b) Available to competitors at least 30 days prior to the event.

24.5.2 Supplementary regulations for Canadian championship rallies are to be:

- (a) Submitted for approval to the National Series Manager at least 90 days prior to the event.
- (b) Submitted to the final version to the CARS President, National Series Manager and Technical Director at least 75 days prior to the event.
- (c) Available to competitors at least 60 days prior to the event.

24.6 Permit

The permit is not to be issued until NRR 24.5.1 and 24.5.2 of these regulations have been complied with unless authorised by the CARS Director of the host region.

24.7 Performance Bonds

In order to ensure that rules and time schedules for National Rallies are complied with, each organiser of such a rally is to submit the appropriate performance bond along with his permit fees. All, part or none of the performance bond is to be returned to the organiser at the discretion of the CARS Board, after consultation with the CARS Office Manager.

25. CANADIAN RALLY CHAMPIONSHIPS

25.1 General

25.1.1 The purpose of the Canadian Rally Championships is to further rally competition by increasing the enthusiasm for, participation in and quality of these events.

25.1.2 Three separate championships are scored: one for drivers, a second for co-drivers and a third for marques.

The CARS President or the person so-named is responsible for scoring the National Rally Championship for Drivers, Co-Drivers and Marques.

25.1.3 The Canadian Rally Championships may comprise events organised by clubs in all regions of CARS and which may be held in areas from the east coast to the west coast of Canada.

25.1.4 The selection of events for the Canadian Rally Championships is made by the CARS Board on the basis of past performance and recommendations of the National Series Manager. Competitors will count all events run if there are six or fewer events in the series. If there are more than six events, competitors may count the number of events run in the championship less 1 (e.g. if 7 events run, minus 1 = 6 events count). The marque championships will count all events in the series.

25.1.5 Awards will be based on point scores obtained in the events. In cases not covered by these regulations, the decision of the CARS Board will be final.

25.1.6 With the exceptions described for specific awards, scoring is as follows:

First	20 pts.	Sixth	6 pts.
Second	15 pts.	Seventh	4 pts.
Third	12 pts.	Eighth	3 pts.
Fourth	10 pts.	Ninth	2 pts.
Fifth	8 pts.	Tenth	1 pt.

In addition, one point is awarded for starting an event, and one point for finishing. These points are not included in the calculation of seed.

25.1.7 Winners shall hold the title "Champion" for the following year.

25.1.8 In the case of a tie for first position in the championships, the competitor with the most event wins shall be declared the winner of the championship. If this fails to break the tie, the competitor with the most second-place finishes shall be declared the winner. If this still fails to break the tie, the competitor with the most third-place finishes shall be declared the winner, etc., etc., until the tie is broken. If this fails to break the tie, then the total number of points earned from all listed events in the Canadian Rally Championship series shall determine the winner. The final tie breaker shall be the total number of Canadian championship events entered that year. Any ties still existing after application of all tie breakers shall remain as ties

25.1.9 Points earned in one championship are not transferable to another championship.

25.1.10 Eligibility. Canadian Rally Licence holders only are eligible and all shall be scored for the Canadian Rally Championships defined in NRR 25.2, 25.3, 25.5, 25.6, 25.7, 25.12, 25.13, 25.14, 25.15 and 25.16. In order for a championship to be awarded, the winner must have entered and started a minimum of 50% of the events in the championship (rounded up if an uneven number of events in the championship year).

25.1.11 Quality token awards, such as trophies, mugs, bowls, plates, etc., must be awarded to the top finishing drivers and co-drivers to a minimum of at least 10% of starters in each event. Organisers are encouraged to offer additional awards as they see fit or are available.

25.2 Canadian Rally Drivers' Championship for the Fidler Trophy

25.2.1 The Fidler Trophy was created in memory of the late Jack Fidler of Kingston, Ontario. He was one of the enthusiasts who hosted the inaugural meeting of the CASC and was a founding member of the St. Lawrence Automobile Club. First prize shall be the Fidler Trophy and award. Keeper awards will be made to fifth place.

25.2.2 Until 1968, this trophy was awarded annually to the person having the most points as either a driver or navigator. Subsequent to the introduction of the Deak Trophy for co-drivers in 1968, the award has been restricted to drivers only.

25.2.3 Previous winners:

1957	Les Chelminski	Montreal, Que.
1957	Les Stanley	Montreal, Que.
1958	Les Chelminski	Montreal, Que.
1959	Art Dempsey	Toronto, Ont.

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1959	William Silvera	Toronto, Ont.
1960	Art Dempsey	Toronto, Ont.
1960	William Silvera	Toronto, Ont.
1961	William Silvera	Toronto, Ont.
1962	John Wilson	Toronto, Ont.
1963	John Bird	Toronto, Ont.
1964	John Bird	Toronto, Ont.
1965	John Bird	Toronto, Ont.
1966	Bruce Simpson	Toronto, Ont.
1967	Paul Manson	Toronto, Ont.
1967	Klaus Ross	Montreal, Que.
1968	Keith Ronald	Guelph, Ont.
1969	Bruce Schmidt	Tavistock, Ont.
1970	Walter Boyce	Ottawa, Ont.
1971	Walter Boyce	Ottawa, Ont.
1972	Walter Boyce	Ottawa, Ont.
1973	Walter Boyce	Ottawa, Ont.
1974	Walter Boyce	Ottawa, Ont.
1975	Jean-Paul Perusse	Laval, Que.
1976	Jean-Paul Perusse	Laval, Que.
1977	Taisto Heinonen	Langley, B.C.
1978	Taisto Heinonen	Langley, B.C.
1979	Taisto Heinonen	Langley, B.C.
1980	Taisto Heinonen	Langley, B.C.
1981	Randy Black	Toronto, Ont.
1982	Taisto Heinonen	Langley, B.C.
1983	Randy Black	Oakville, Ont.
1984	Tim Bendle	Spruce Grove, Alta.
1985	Tim Bendle	Spruce Grove, Alta.
1986	Bo Skowronnek	Saskatoon, Sask.
1987	Alain Bergeron	Lac St. Charles, Que.
1988	Alain Bergeron	Lac St. Charles, Que.
1989	Paul Choiniere	South Burlington, VT.
1990	Tom McGeer	Montreal, Que.
1991	Frank Sprongl	Woodbridge, Ont.
1992	Tom McGeer	Brampton, Ont.
1993	Tom McGeer	Georgetown, Ont.
1994	Frank Sprongl	Woodbridge, Ont.
1995	Frank Sprongl	Mississauga, Ont.
1996	Carl Merrill	Ogunquit, ME
1997	Frank Sprongl	Mississauga, Ont. (Performance)
	Peter Watt	Peterborough, Ont. (Navigational)
1998	Frank Sprongl	Mississauga, Ont.
1999	Frank Sprongl	Mississauga, Ont.
2000	Tom McGeer	Georgetown, Ont. (Performance)
	Paul Henshall	Brampton, Ont. (Navigational)
2001	Tom McGeer	Georgetown, Ont.
2002	Patrick Richard	Vancouver, B.C.
2003	Tom McGeer	Georgetown, Ont.
2004	Patrick Richard	Garibaldi Highlands, B.C.
2005	Peter Thomson	Toronto, Ont.
2006	Antoine L'Estage	St-Jean-Sur-Richelieu, Que
2007	Antoine L'Estage	St-Jean-Sur-Richelieu, Que
2008	Patrick Richard	Squamish, B.C.
2009	Patrick Richard	Squamish, B.C.
2010	Antoine L'Estage	St-Jean-Sur-Richelieu, Que
2011	Antoine L'Estage	St-Jean-Sur-Richelieu, Que
2012	Antoine L'Estage	St-Jean-Sur-Richelieu, Que
2013	Antoine L'Estage	St-Jean-Sur-Richelieu, Que

25.3 Canadian Rally Co-Drivers' Championship for the Deak Trophy

25.3.1 This bronze sculpture is the work of Ed Deak, an active navigator himself for many years. The donation of this award is his way of honouring Canada's best rally navigators and co-drivers; to keep their names remembered as long as sportsmen take to the backroads of this great country in search of competition and adventure. Awards will be the Deak Trophy and award for first place, plus keeper awards to fifth place by CARS.

25.3.2 Previous winners:

1968	John Slade	Ottawa, Ont.
1969	Betty Schmidt	Tavistock, Ont.
1969	Paul Manson	Toronto, Ont.
1970	Doug Woods	Ottawa, Ont.

1971	Doug Woods	Ottawa, Ont.
1972	Doug Woods	Ottawa, Ont.
1973	Doug Woods	Ottawa, Ont.
1974	Doug Woods	Ottawa, Ont.
1975	John Bellefleur	Toronto, Ont.
1976	John Bellefleur	Toronto, Ont.
1977	Tom Burgess	Burnaby, B.C.
1978	Tom Burgess	Burnaby, B.C.
1979	Tom Burgess	Burnaby, B.C.
1980	Tom Burgess	Burnaby, B.C.
1981	Bob Lee	Milton, Ont.
1982	Tom Burgess	Tampa, FL
1983	Tom Burgess	Tampa, FL
1984	Mary Crundwell	Fonthill, Ont.
1985	Louis Belanger	St. Laurent, Que.
1986	Terry Epp	Stouffville, Ont.
1987	Martin Headland	Ottawa, Ont.
1988	Raymond Cadieux	Pickering, Ont.
1989	Martin Headland	Calgary, Alta.
1990	Trish Sparrow	Bramalea, Ont.
1991	Dan Sprongl	Woodbridge, Ont.
1992	Trish Sparrow	Brampton, Ont.
1993	Trish Sparrow	Georgetown, Ont.
1994	Dan Sprongl	Woodbridge, Ont.
1995	Mike Koch	Nepean, Ont.
1996	Yorgi Bittner	Plain City, OH
1997	Dan Sprongl	Mississauga, Ont. (Performance)
	Jenny MacKenzie	Peterborough, Ont. (Navigational)
1998	Dan Sprongl	Mississauga, Ont.
1999	Dan Sprongl	Mississauga, Ont.
2000	Mark Williams	North Potomac, MD (Performance)
	Robert Roaldi	Thornhill, Ont. (Navigational)
2001	Mark Williams	North Potomac, MD
2002	Ian McCurdy	Whistler, B.C.
2003	Philip Erickson	Gatineau, Que.
2004	Nathalie Richard	Halifax, N.S.
2005	Rod Hendricksen	Clinton, NJ
2006	Ole Holter	Long Beach, CA
2007	Nathalie Richard	St-Jean-Sur-Richelieu, Que
2008	Alan Ockwell	Toronto, Ont.
2009	Alan Ockwell	Toronto, Ont.
2010	Nathalie Richard	St-Jean-Sur-Richelieu, Que
2011	Nathalie Richard	Halifax, N.S.
2012	Nathalie Richard	Halifax, N.S.
2013	Rob Fagg	Kirk, Isle of Man

25.4 Canadian Rally Championship for Marques

- 25.4.1 The Rally Championship for Marques, which will count all listed events, is designed to stimulate more interest in and support of rallies by manufacturers. The winning manufacturer will receive the trophy to be held for one year, plus a keeper award.
- 25.4.2 If the winning make of automobile is not manufactured in Canada, the trophy will be received by the appointed distributor or agent.
- 25.4.3 On a Canadian Championship Rally, a marque will receive the number of points earned by the highest placing automobile of that make.
- 25.4.4 Points will be awarded on the scale defined in NRR 25.1.6.
- 25.4.5 The car need not be the same model or year to be considered the same marque (i.e.: this championship will determine the make of automobile winning the most rallies such as Renault versus Chevrolet versus Ford, etc., and not Renault 10 versus Renault 12 versus Renault 16, etc.).
- 25.4.6 Previous winners:

1963	Volkswagen	1978	Toyota	1993	Subaru	2008	Subaru
1964	Volkswagen	1979	Toyota	1994	Audi	2009	Subaru
1965	Volkswagen	1980	Toyota	1995	Audi	2010	Subaru
1966	Volvo	1981	Toyota	1996	Eagle	2011	Mitsubishi
1967	Datsun	1982	Toyota	1997	Audi	2012	Mitsubishi
1968	Volkswagen	1983	Nissan	1998	Audi	2013	Mitsubishi
1969	Datsun	1984	Toyota	1999	Audi		
1970	Datsun	1985	Toyota	2000	Subaru		

1971	Datsun	1986	Volvo	2001	Subaru
1972	Datsun	1987	Toyota	2002	Subaru
1973	Fiat	1988	Toyota	2003	Subaru
1974	Toyota	1989	Audi	2004	Subaru
1975	Fiat	1990	Audi	2005	Subaru
1976	Fiat	1991	Audi	2006	Hyundai
1977	Toyota	1992	Audi	2007	Hyundai

25.5 2WD Canadian Rally Championship

25.5.1 This overall two wheel drive (2WD) championship recognises those who are competing in the Canadian Rally Championship in a 2WD vehicle. There will be separate championships scored for drivers, co-drivers and marques.

25.5.2 Scoring for drivers and co-drivers is based on a maximum of four (4) events with no more than two from each of two zones (east and west), as determined by the Ontario/Manitoba provincial boundary, being counted. The marque championship will count all events in the series. Points and standings will be awarded, based on event overall 2WD finishing position (regardless of vehicle class) as per 25.1.6 save that winner (driver and co-driver) must have earned points in both zones as defined above. In a case of a tie for first position in the championships, the tie breaker 25.1.8 regarding the number of firsts, seconds, thirds, etc. is applied to the 4 events counted in calculating the tied score and that only the last two tie breakers determinates apply to the total number of events in the series.

25.5.3 Awards will be presented to the winning driver, co-driver and marque by CARS.

25.5.4 Previous winners:

Driver:			
	2009	Jan Zedril	Winnipeg, Man.
	2010	Jan Zedril	Winnipeg, Man.
	2011	Jan Zedril	Winnipeg, Man.
	2012	Eric Grochowski	Calgary, AB.
	2013	Paul Hartl	Toronto, Ont.
Co-Driver:			
	2009	Leanne Junnila	Calgary, AB.
	2010	Jody Zedril	Winnipeg, Man.
	2011	Jody Zedril	Winnipeg, Man.
	2012	Leanne Junnila	Calgary, AB.
	2013	Chuck Story	North Gower, Ont.
Marques:			
	2009	Mitsubishi	
	2010	Mitsubishi	
	2011	Mitsubishi	
	2012	Volkswagen	
	2013	Volkswagen	

25.6 Canadian Production Rally Drivers' Championship for the Andy Browning Memorial Trophy

25.6.1 The championship is to encourage drivers to participate in the Canadian Rally Championship in basically stock vehicles and is dedicated to the memory of Andy Browning whose talent, skills and zest for competition in this class won him the recognition and admiration of all his peers.

- (a) Two separate championships will be scored. (For class descriptions see NRR 12.8).
- (b) From 1977 through 1981 there were no displacement classes in the championship. From 1982 through 1988 production class was divided into two displacement classes: production A for cars with engines of 1600 cc displacement or less and production B for cars with engines with greater than 1600 cc displacement. From 1989 to 1991, production class was divided into two displacement classes: Production, for cars with engines of 2350 cc displacement or less, and Production GT for cars with engines with greater than 2350 cc displacement. From 1992 to 2000, production class was divided into three displacement classes: Production 1750, for cars with engines of 1750 cc displacement or less, Production Sport for cars with engines of 2650 cc displacement or less, and Production GT for cars with engines with greater than 2650 cc displacement. From 2001 through 2008, production class was divided into four displacement classes, P1, less than 1400cc; P2, 1402cc to 1600cc; P3, 1601cc to 2000cc and P4, more than 2000cc. From 2002 through 2007, Group N classes were also added to the production championship. From 2009 through 2012, production class was divided into two displacement classes: Production Sport with an adjusted displacement of 2000cc or less and Production Sport with an adjusted displacement greater than 2000cc.

25.6.2 Sections 12.8 and 25.1 of this book shall be observed in this championship, except for the following:

Scoring is per the following points schedule:

First	9 pts.	Fourth	3 pts.
Second	6 pts.	Fifth	2 pts.
Third	4 pts.	Sixth	1 pt.

25.6.3 Awards will be presented to the winners by CARS.

25.6.4 Previous winners:

1977	Lauri Paivarinta	Kelowna, B.C.	
1978	Andy Falkiewicz	Brantford, Ont.	
1979	John Nixon	Calgary, Alta.	
1980	Niall Leslie	Mississauga, Ont.	
1981	Niall Leslie	Mississauga, Ont.	
1982	Walter Boyce	Munster, Ont.	Prod A
	Niall Leslie	Mississauga, Ont.	Prod B
1983	Michel Poirier-Defoy	Lorraine, Que.	Prod A
	Tom Novak	Toronto, Ont.	Prod B
1984	Michel Poirier-Defoy	Lorraine, Que.	Prod A
	Francois Arbique	Montreal, Que.	Prod B
1985	Michel Poirier-Defoy	Lorraine, Que.	Prod A
	Shawn Bishop	Calgary, Alta.	Prod B
1986	Andre Normandin	Longueuil, Que.	Prod A
	Shawn Bishop	Calgary, Alta.	Prod B
1987	Alain Bergeron	Lac St. Charles, Que.	Prod A
	Thierry Menegoz	Montreal, Que.	Prod B
1988	Niall Leslie	Gananoque, Ont.	Prod A
	Alain Bergeron	Lac St. Charles, Que.	Prod B
1989	Philippe Dube	Boie des Filion, Que.	Prod
	Shawn Bishop	Calgary, Alta.	Prod GT
1990	Barry Latreille	Williamstown, Ont.	Prod
	Shawn Bishop	Calgary, Alta.	Prod GT
1991	Sylvain Vincent	Laplaine, Que.	Prod
	Yves Barbe	Lavalle, Que.	Prod GT
1992	Ari Montanen	Richmond Hill, Ont.	Prod 1750
	Barry Latreille	Williamstown, Ont.	Prod Sp
	Tom McGeer	Brampton, Ont.	Prod GT
1993	Walter Peterson	Olds, Alta.	Prod 1750
	Barry Latreille	Williamstown, Ont.	Prod Sp
	Tom McGeer	Georgetown, Ont.	Prod GT
1994	Jean-Philippe Chevarier	St. Bruno, Que.	Prod 1750
	Walt Petersen	Olds, Alta.	Prod Sp
	Barry Latreille	Williamstown, Ont.	Prod GT
1995	Ari Montonen	Richmond Hill, Ont.	Prod 1750
	Bob Elliott	Rutland, VT	Prod Sp
	Sylvain Vincent	La Plaine, Que.	Prod GT
1996	Michel Lavoie	La Baie, Que.	Prod 1750
	Pete Pollard	Etobicoke, Ont.	Prod Sp
	Jorge Dascollas	Calgary, Alta.	Prod GT
1997	Marc Racine	Rawdon, Que.	Prod 1750
	Ari Montonen	Richmond Hill, Ont.	Prod Sp
	Sylvain Vincent	La Plaine, Que.	Prod GT
1998	Steve Walkington	Hudson, Que.	Prod 1750
	Owen Cule	Scarborough Ont.	Prod 1750
	Pete Pollard	Etobicoke, Ont.	Prod Sp
	John Paynter	Lower Sackville, N.S.	Prod GT
1999	Ilana Rosenshein	Mississauga, Ont.	Prod 1750
	Bruno Laverdiere	Laval, Que.	Prod Sp
	Sylvain Vincent	La Plaine, Que.	Prod GT
2000	Mark Krawczyk	Mississauga, Ont.	Prod 1750
	Scott Comens	Kitchener, Ont.	Prod Sp
	Patrick Richard	Vancouver, BC	Prod GT
2001	Stephane McNeil	Prevost, Que.	P1
	Stephane Levesque	Montreal, Que.	P2
	Steve Walkington	Hudson, Que.	P3
	John Paynter	Lower Sackville, N.S.	P4
	Jean-Sebastien Besner	Montreal, Que.	N4
2002	Yavor Klostranec	Newmarket, Ont.	P2
	Steve Walkington	Hudson, Que.	P3
	Joel Levac	Riviere Baudette, Que.	P4
	Patrick Richard	Vancouver, B.C.	N4
2003	Eric Lapointe	Sherbrooke Que.	P1
	Scott Comens	Kitchener, Ont.	P3
	Julien Pilon	Brownsburg Chatham, Que.	P4
	Rejean Losier	St Sauveur des Monts, Que.	N4
2004	William Bacon	Chelmsford, Mass.	P2

	Vittorio Bares	Candia, N.H.	P3
	Frederic Labrie	Beauport, Que.	P4
	Rejean Losier	St Sauveur des Monts, Que.	N4
2005	Andre Cloutier	L'Ange-Gardien, Que.	P1
	Frank Sprongl	Georgetown, Ont.	P2
	Frederic Labrie	Beauport, Que.	P4
	Peter Thomson	Toronto, Ont.	N4
2006	Christian Gagne	Québec City, Que.	P1
	Frank Sprongl	Georgetown, Ont.	P2
	Norm LeBlanc	Pemberton, BC	P4
2007	Jim Stevens	Kingsville, Ont.	P1
	Rob MacKenzie	Bond Head, Ont.	P2
	Martin Gauvin	New Richmond, Que.	P4
2008	Jim Stevens	Kingsville, Ont.	P2
	Jan Zedril	Winnipeg, Man.	P3
	Ted Mendham	Lyndeborough, N.H.	P4
2009	Neil Wright	Dunrobin, Ont.	Prod Sp
	Hardy Schmidtke	Cochrane, Alta.	Prod GT
2010	Jeffrey Dowell	Windsor, Ont.	Prod Sp
	Warren Haywood	Chelsea, Que.	Prod GT
2011	Jeffrey Dowell	Windsor, Ont.	Prod Sp
	Alexandre Rochon-Ouellette	Gatineau, Que.	Prod GT
2012	Not awarded		Prod Sp
	Alexandre Rochon-Ouellette	Gatineau, Que.	Prod GT
2013	Éric Deschênes	Repentigny, Que.	P2WD
	Jeremy Norris	Kitchener, Ont.	P4WD

25.7 Canadian Rally Championship for Production Co-Drivers

25.7.1 This championship recognises the co-drivers of production class and regulations are as per NRR 25.1 and 25.6 above.

25.7.2 Previous winners:

1977	Marcel Paul Raymond	Montreal, Que.	
1978	Angus Chisholm	Ottawa, Ont.	
1979	Robin Edwards	Pierrefonds, Que.	
1980	Fred Baker	West Hill, Ont.	
1981	Martin Headland	Surrey, B.C.	
1982	Robin Edwards	Pierrefonds, Que.	Prod A
	Don MacDougall	Edmonton, Alta.	Prod B
1983	Doug Ayles	Toronto, Ont.	Prod A
	Susan Metcalfe	Toronto, Ont.	Prod B
1984	Francois Cyr	Bellefeuille, Que.	Prod A
	Marcel Paul Raymond	Montreal, Que.	Prod B
1985	Francois Cyr	Montreal, Que.	Prod A
	Suzanne Stewart	Calgary, Alta.	Prod B
1986	Louis Belanger	St. Laurent, Que.	Prod A
	Suzanne Stewart	Calgary, Alta.	Prod B
1987	Gilles Lacroix	Ste-Monique, Que.	Prod A
	James Dowell	Orleans, Ont.	Prod B
1988	Brian Maxwell	Toronto, Ont.	Prod A
	Raymond Cadieux	Pickering, Ont.	Prod B
1989	Gilles Lacroix	Ste-Monique, Que.	Prod
	Suzanne Stewart	Calgary, Alta.	Prod GT
1990	Ross Wood	Milton, Ont.	Prod
	Suzanne Stewart	Calgary, Alta.	Prod GT
1991	Paul Turgeon	St. Lin, Que.	Prod
	Gilles Lacroix	Ste-Monique, Que.	Prod GT
1992	Brian Maxwell	Port Hope, Ont.	Prod 1750
	Sandra Latreille	Williamstown, Ont.	Prod Sp
	Trish Sparrow	Brampton, Ont.	Prod GT
1993	Harry Pressey	St. Thomas, Ont.	Prod 1750
	Lisa Pelling	Hudson, Que.	Prod 1750
	Sandra Latreille	Williamstown, Ont.	Prod Sp
	Trish Sparrow	Georgetown, Ont.	Prod GT
1994	Shirley Normandin	St. Bruno, Que.	Prod 1750
	Harry Pressey	St. Thomas, Ont.	Prod Sp
	Trish Sparrow	Georgetown, Ont.	Prod GT
1995	Martyn Appleford	North York, Ont.	Prod 1750
	Jill Maynard	Rutland, VT	Prod Sp
	Dominique Cyr	La Plaine, Que.	Prod GT
1996	Eric Tremblay	St. Jean Chrysostom, Que.	Prod 1750
	Jill Maynard	Rutland, VT	Prod Sp

	Steve Walkington	Hudson, Que.	Prod GT
1997	Luc Piche	Boisbriand, Que.	Prod 1750
	Brian Maxwell	Port Hope, Ont.	Prod Sport
	Dominique Cyr	La Plaine, Que.	Prod GT
1998	Peter Watt	Peterborough, Ont.	Prod 1750
	Tracey Smith-Pollard	Etobicoke, Ont.	Prod Sp
	Clarke Paynter	Dartmouth, N.S.	Prod GT
1999	Arthur Trojanowicz	Toronto, Ont.	Prod 1750
	Martin Duval	St-Catherine, Que.	Prod Sp
	Dominique Cyr	La Plaine, Que.	Prod GT
2000	Artur Trojanowicz	Toronto, Ont.	Prod 1750
	Harry Pressey	St. Thomas, Ont.	Prod Sp
	Ian McCurdy	Whstler, BC	Prod GT
2001	Annie Duranceau	Prevost, Que.	P1
	Jerry Plourde	New Richmond, Que.	P2
	Hytham Mokhtar	Kirkland, Que.	P3
	Clarke Paynter	Dartmouth, N.S.	P4
	Jacques Besner	Montreal, Que.	N4
2002	Jessie Klostranec	Newmarket, Ont.	P2
	Eric Bourbonnais	Montreal, Que.	P4
	Ian McCurdy	Whistler, B.C.	N4
2003	Sebastien Lefebvre	Levis, Que.	P1
	Sandy Hygate	Mississauga, Ont.	P3
	Marie-Eve Pilon	Brownsburg Chatham, Que.	P4
	Art Losier	Ste Julie, Que.	N4
2004	Peter Watt	Peterborough, Ont.	P2
	Sandy Hygate	Mississauga, Ont.	P3
	Robert Labrie	Beauport, Que.	P4
	Art Losier	Ste Julie, Que.	N4
2005	Peter Watt	Peterborough, Ont.	P1
	Dan Sprogle	Mississauga, Ont.	P2
	Robert Labrie	Montmagny, Que.	P4
	Rod Hendricksen	Clinton, NJ	N4
2006	Jérôme Sanfaçon	Québec City, Que.	P1
	Nancy Beaulieu	Beauport, Que.	P2
	Keith Morison	Calgary, AB	P4
2007	Rebecca Stevens	Windsor, Ont.	P1
	Greg James	Port Hope, Ont.	P2
	Daniel Duchesneau	New Richmond, Que.	P4
2008	Rebecca Stevens	Windsor, Ont.	P2
	Jody Zedril	Winnipeg, Man.	P3
	Lise Mendham	Lyndeborough, N.H.	P4
2009	Lorna Wright	Dunrobin, Ont.	Prod Sp
	Adam Vokes	Red Der, AB.	Prod GT
2010	Yavor Klostranec	Newmarket, Ont.	Prod Sp
	Not Awarded		Prod GT
2011	Yavor Klostranec	Newmarket, Ont.	Prod Sp
	Nicolas Rochon-Ouellette	Gatineau, Que.	Prod GT
2012	Not awarded		Prod Sp
	Nicolas Rochon-Ouellette	Gatineau, Que.	Prod GT
2013	Catherine Asselin	Pointe-Claire, Que.	P2WD
	Andrew Avery	Beaumont, AB.	P4WD

25.8 Canadian Rally Championship for Production Marques

25.8.1 This championship is to encourage manufacturers to participate with cars as in NRR 25.6.1. above. Regulations are as in NRR 25.4.1, 25.4.2, 25.4.3 and 25.4.5 and NRR 25.6.1, 25.6.2 and 25.6.3.

25.8.2 In each rally, the marque will receive the number of points earned by the highest placing vehicle of that marque in its class.

25.8.3 Previous winners:

1977	Renault	2000	Subaru	(Prod 1750)
1978	Datsun		Volkswagen	(P. Sport)
1979	Saab		Subaru	(Prod GT)
1980	Datsun	2001	Subaru	(P1)
1981	Datsun		not awarded	(P2)
1982	Honda		Volkswagen	(P3)
	Datsun		Subaru	(P4)
1983	Toyota		Mitsubishi	(N4)
	Toyota	2002	Subaru	(P1)
1984	Toyota		Nissan	(P2)
	Toyota		Volkswagen	(P3)

1985	Toyota	(Prod A)		Subaru	(P4)
	Toyota	(Prod B)		Subaru	(N4)
1986	Toyota	(Prod A)	2003	Subaru	(P1)
	Volkswagen	(Prod B)		Toyota	(P2)
1987	Toyota	(Prod A)		Volkswagen	(P3)
	Dodge	(Prod B)		Subaru	(P4)
1988	Toyota	(Prod A)		Mitsubishi	(N4)
	Toyota	(Prod B)	2004	Suzuki	(P1)
1989	Toyota	(Prod)		Suzuki	(P2)
	Mazda	(Prod GT)		Ford	(P3)
1990	Toyota	(Prod)		Subaru	(P4)
	Mazda	(Prod GT)		Subaru	(N4)
1991	Toyota	(Prod)	2005	Suzuki	(P1)
	Eagle	(Prod GT)		Suzuki	(P2)
1992	Suzuki	(Prod 1750)		Volkswagen	(P3)
	Ford	(P. Sport)		Subaru	(P4)
	Subaru	(Prod GT)		Subaru	(N4)
1993	Suzuki	(Prod 1750)	2006	Subaru	(P1)
	Ford	(P. Sport)		Suzuki	(P2)
	Subaru	(Prod GT)		Daimler-Chrysler	(P3)
1994	Suzuki	(Prod 1750)		Subaru	(P4)
	Honda	(P. Sport)	2007	Suzuki	(P1)
	Eagle	(Prod GT)		Honda	(P2)
1995	Lada	(Prod 1750)		Mazda	(P3)
	Volkswagen	(P. Sport)		Subaru	(P4)
	Eagle	(Prod GT)	2008	Not Awarded	(P1)
1996	Suzuki	(Prod 1750)		Suzuki	(P2)
	Volkswagen	(P. Sport)		Mitsubishi	(P3)
	Eagle	(Prod GT)		Subaru	(P4)
1997	Honda	(Prod 1750)	2009	Mazda	(P. Sport)
	Honda	(P. Sport)		Subaru	(Prod GT)
	Eagle	(Prod GT)	2010	Mitsubishi	(P. Sport)
1998	Honda	(Prod 1750)		Subaru	(Prod GT)
	Volkswagen	(P. Sport)	2011	Mitsubishi	(P. Sport)
	Subaru	(Prod GT)		Subaru	(Prod GT)
1999	Suzuki	(Prod 1750)	2012	Mitsubishi	(P. Sport)
	Toyota	(Prod Sp)		Subaru	(Prod GT)
	Subaru	(Prod GT)	2013	Mitsubishi	P2WD
				Subaru	P4WD

25.9 Canadian Rally Championship for Open Class Drivers

25.9.1 This championship recognises those who are competing in the Canadian Rally Championship in open class vehicles and was scored from 1987 to 2011.

From 1987 through 1989, this championship was for FIA Group A drivers.

25.9.2 Previous winners:

1987	Bo Skowronnek	Saskatoon, Sask.	(Group A)
1988	Tim O'Neil	Whitefield, NH	(Group A)
1989	Demetrius Andreou	Toronto, Ont	(Group A)
1990	Frank Sprongl	Woodbridge, Ont.	
1991	Frank Sprongl	Woodbridge, Ont.	
1992	Frank Sprongl	Woodbridge, Ont.	
1993	Tim Bendle	Calgary, Alta.	
1994	Frank Sprongl	Woodbridge, Ont.	
1995	Frank Sprongl	Mississauga, Ont.	
1996	Carl Merrill	Ogunquit, ME	
1997	Frank Sprongl	Mississauga, Ont.	
1998	Frank Sprongl	Mississauga, Ont.	
1999	Frank Sprongl	Mississauga, Ont.	
2000	Tom McGeer	Georgetown, Ont.	
2001	Tom McGeer	Georgetown, Ont.	
2002	Andrew Comrie-Picard	Edmonton, Alta.	
2003	Tom McGeer	Georgetown, Ont.	
2004	Patrick Richard	Garibaldi Highlands, BC.	
2005	Antoine L'Estage	St.Jean-sur-Richelieu, Que.	
2006	Antoine L'Estage	St.Jean-sur-Richelieu, Que.	
2007	Antoine L'Estage	St.Jean-sur-Richelieu, Que.	
2008	Patrick Richard	Squamish, BC.	
2009	Patrick Richard	Squamish, BC.	
2010	Antoine L'Estage	St.Jean-sur-Richelieu, Que.	
2011	Antoine L'Estage	St.Jean-sur-Richelieu, Que.	

25.10 Canadian Rally Championship for Open Class Co-Drivers

25.10.1 This Championship recognises those who are competing in the Canadian Rally Championship in open class vehicles and regulations are as per NRR 25.9.1 above. The championship was scored from 1987 through 2011.

From 1987 through 1989, this championship was for FIA Group A co-drivers.

25.10.2 Previous winners:

1987	Terry Epp	Stouffville, Ont	(Group A)
1988	Martin Headland	Calgary, Alta	(Group A)
1989	David Gill	Toronto, Ont	(Group A)
1990	Dan Sprongl	Woodbridge, Ont.	
1991	Dan Sprongl	Woodbridge, Ont.	
1992	Dan Sprongl	Woodbridge, Ont.	
1993	Chris Gielas	Don Mills, Ont.	
1994	Dan Sprongl	Woodbridge, Ont.	
1995	John Bellefleur	Thornhill, Ont.	
1996	John Bellefleur	Thornhill, Ont.	
1997	Dan Sprongl	Mississauga, Ont.	
1998	Dan Sprongl	Mississauga, Ont.	
1999	Dan Sprongl	Mississauga, Ont.	
2000	Mark Williams	Potomac, MD	
2001	Mark Williams	Potomac, MD	
2002	David Shindle	Falls Church, VA	
2003	Philip Erickson	Gatineau, Que.	
2004	Nathalie Richard	Halifax, N.S.	
2005	Yanick Napert	St.Jean-sur-Richelieu, Que.	
2006	Ole Holter	Long Beach, CA	
2007	Nathalie Richard	St-Jean-Sur-Richelieu, Que.	
2008	Alan Ockwell	Toronto, Ont.	
2009	Alan Ockwell	Toronto, Ont.	
2010	Nathalie Richard	St-Jean-Sur-Richelieu, Que.	
2011	Nathalie Richard	Halifax, N.S.	

25.11 Canadian Rally Championship for Open Class Marques

25.11.1 This Championship is to encourage manufacturers to participate with cars in the Open Class. Regulations are as in NRR 25.4.1, 25.4.3 and 25.4.5 and NRR 25.9.1. The championship was scored from 1987 through 2011.

From 1987 through 1989, this championship was for FIA Group A Marques.

25.11.2 In each rally, the Marque received the number of points earned by the highest placing vehicle of that Marque in Open Class.

25.11.3 Previous winners:

1987	Volvo	(Group A)	2002	Subaru
1988	Volkswagen	(Group A)	2003	Mitsubishi
1989	Audi	(Group A)	2004	Subaru
1990	Audi	(Open)	2005	Subaru
1991	Audi		2006	Hyundai
1992	Audi		2007	Hyundai
1993	Ford		2008	Subaru
1994	Audi		2009	Subaru
1995	Audi		2010	Mitsubishi / Subaru (tie)
1996	Eagle		2011	Mitsubishi
1997	Audi			
1998	Audi			
1999	Audi			
2000	Subaru			
2001	Subaru			

25.12 Canadian Rally Championship for Open 2WD Drivers

25.12.1 This championship recognises those drivers who are competing in the Canadian Rally Championship in Open 2WD vehicles. From 1995 through 2012, the class was called Group 2 for 2WD vehicles with an adjusted displacement no greater than 2400cc.

25.12.2 Sections NRR 25.1 shall be observed in this championship except for scoring which shall be as per NRR 25.6.2.

25.12.3 Awards will be presented to the winners by CARS.

25.12.4 Previous winners:

1995	Jon Nichols	Baie D'Urfe, Que.
1996	Chris Havas	Richmond, VT
1997	Jon Nichols	Baie D'Urfe, Que.
1998	Jon Nichols	Baie D'Urfe, Que.
1999	Jon Nichols	Baie D'Urfe, Que.
2000	Karoly Toth	Mississauga, Ont.
2001	Gord Olsen	Brooks, Alta.
2002	Gord Olsen	Brooks, Alta.
2003	Gord Olsen	Brooks, Alta.
2004	Jon Nichols	Lachine, Que.
2005	Jon Nichols	Lachine, Que.
2006	Maxime Labrie	La Prairie, Que.
2007	Colin Armstrong	Calgary, Alta.
2008	Eric Grochowski	Calgary, Alta.
2009	Jan Zedril	Winnipeg, Man.
2010	Jan Zedril	Winnipeg, Man.
2011	Simon Dubé	Jonquière, Que.
2012	Eric Grochowski	Calgary, Alta.
2013	Paul Hartl	Toronto, Ont.

25.13 Canadian Rally Championship for Group Two Co-Drivers

25.13.1 This championship recognises those co-drivers who are competing in the Canadian Rally Championship in Group 2 cars.

25.13.2 Sections NRR 25.1 shall be observed in this championship except for scoring which shall be as per NRR 25.6.2.

25.13.3 Awards will be presented to the winners by CARS.

25.13.4 Previous winners:

1995	Mike Koch	Nepean, Ont.
1996	Mike Koch	Nepean, Ont.
1997	Mike Koch	Nepean, Ont.
1998	Mike Koch	Nepean, Ont.
1999	Mike Koch	Nepean, Ont.
2000	Tom Bartman	Markham, Ont.
2001	Kathy Olsen	Brooks, Alta.
2002	Kathy Olsen	Brooks, Alta.
2003	Dave Shindle	Falls Church, VA
2004	Carl Schenk	Montreal, Que.
2005	Carl Schenk	Ste.Anne-de-Bellvue, Que.
2006	Jean-François Guité	Québec City, Que.
2007	Ryan Warrington	Calgary, Alta.
2008	Leanne Junnila	Calgary, Alta.
2009	Jody Zedril	Winnipeg, Man.
2010	Jody Zedril	Winnipeg, Man.
2011	Pat Lavigne	Mirabel, Que.
2012	Leanne Junnila	Calgary, Alta.
2013	Chuck Story	North Gower, Ont.

25.14 Canadian Rally Championship for Group Two Marques

25.14.1 This championship is to encourage manufacturers to participate with cars in Group Two Class.

25.14.2 Sections NRR 25.1 shall be observed in this championship except for scoring which shall be as per NRR 25.6.2.

25.14.3 In each rally the Marque will receive the number of points earned by the highest placing vehicle of that marque in Group Two.

25.14.4 Previous winners:

1997	Volkswagen
1998	Volkswagen
1999	Volkswagen
2000	Nissan
2001	Volkswagen
2002	Volkswagen
2003	Volkswagen

2004	Volkswagen
2005	Volkswagen
2006	Honda
2007	Volkswagen
2008	Volkswagen
2009	Mitsubishi
2010	Mitsubishi
2011	Volkswagen
2012	Volkswagen
2013	Volkswagen

25.15 Canadian Rally Championship for Group Five Drivers

25.15.1 This championship recognises those co-drivers who are competing in the Canadian Rally Championship in Group Five cars.

25.15.2 Sections NRR 25.1 shall be observed in this championship except for scoring which shall be as per NRR 25.6.2.

25.15.3 Awards will be presented to the winners by CARS.

25.15.4 Previous winners:

2006	Zbigniew Szewczyk	Calgary, AB.
2007	Martin Walter	North Gower, Ont.
2008	Martin Walter	North Gower, Ont.
2009	not awarded	
2010	not awarded	
2011	Jan Zedril	Winnipeg, Man.
2012	not awarded	
2013	not awarded	

25.16 Canadian Rally Championship for Group Five Co-Drivers

25.16.1 This championship recognises those drivers who are competing in the Canadian Rally Championship in Group Five cars.

25.16.2 Sections NRR 25.1 shall be observed in this championship except for scoring which shall be as per NRR 25.6.2.

25.16.3 Awards will be presented to the winners by CARS.

25.16.4 Previous winners:

2006	Tomasz Karzynski	Calgary, AB.
2007	Ken Lagarec	Gatineau, Que
2008	Ferdinand Trauttmansdorff	Nepean, Ont
2009	not awarded	
2010	not awarded	
2011	Jody Zedril	Winnipeg, Man.
2012	not awarded	
2013	not awarded	

25.17 Canadian Rally Championship for Group Five Marques

25.17.1 This championship is to encourage manufacturers to participate with cars in Group Five Class.

25.17.2 Sections NRR 25.1 shall be observed in this championship except for scoring which shall be as per NRR 25.6.2.

25.17.3 In each rally the Marque will receive the number of points earned by the highest placing vehicle of that marque in Group Five.

25.17.4 Previous winners:

2006	Mitsubishi
2007	Nissan
2008	Nissan
2009	Nissan
2010	not awarded
2011	Mitsubishi
2012	Nissan
2013	not awarded

25.18 Ladies Rally Championship for the Joan McAlpine Memorial Trophy

25.18.1 The Joan McAlpine Trophy is awarded to promote an interest in rallying among the lady members of the sport. Scoring regulations for this trophy are as follows:

- (a) It is not necessary to be a member of an all-lady crew to score points earned in qualifying events.
- (b) The driver and co-driver shall receive equal points.
- (c) Points shall be awarded as in NRR 25.6.2, above.
- (d) Events qualifying shall be in the Canadian Rally Championship.

25.18.2 Previous winners:

1962	Ann Meyers	Toronto, Ont.
1963	Ann Coomb	Toronto, Ont.
1964	Barbara J. Jack	Milton, Ont.
1965	Betty Schmidt	Tavistock, Ont.
1966	Marilyn Maxwell	Willowdale, Ont.
1967	Marilyn Maxwell	Willowdale, Ont.
1968	Katherine Edmond	Ottawa, Ont.
1969	Betty Schmidt	Tavistock, Ont.
1970	Betty Schmidt	Tavistock, Ont.
1971	not presented	
1972	not presented	
1973	Colleen Hughes	Montreal, Que.
1974	Connie Moser	St. Laurent, Que.
1975	Brenda Carpenter	Oakville, Ont.
1976	Cathrine Skowronnek	Saskatoon, Sask.
1977	Vicki	Burlington, VT.
1978	Gale Berry	St. Catharines, Ont.
1979	Lynn Nixon	Calgary, Alta.
1980	Cathy Pitman	Calgary, Alta.
1981	Cynthia Lomheim	Calgary, Alta.
1982	Lynn Nixon	Calgary, Alta.
1983	Suzanne Stewart	Calgary, Alta.
1984	Mary Crundwell	Fonthill, Ont.
1985	Suzanne Stewart	Calgary, Alta.
1986	Suzanne Stewart	Calgary, Alta.
1987	Christine Chan	Scarborough, Ont
1988	Betty-Ann Gilliland	Ann Arbor, MI
1989	Suzanne Stewart	Calgary, Alta.
1990	Trish Sparrow	Bramalea, Ont.
1991	Sandra Latreille	Williamstown, Ont.
1992	Trish Sparrow	Brampton, Ont.
1993	Trish Sparrow	Georgetown, Ont.
1994	Sandra Latreille	Williamstown, Ont.
1995	Dominique Cyr	La Plaine, Que.
	Sandra Latreille	Williamstown, Ont.
1996	Sandra Latreille	Williamstown, Ont.
1997	Sandra Latreille	Williamstown, Ont.
1998	Trish McGeer	Georgetown, Ont.
1999	Dominique Cyr	La Plaine, Que.
2000	Dominique Cyr	La Plaine, Que.
2001	Kathy Olsen	Brooks, Alta.
2002	Kathy Olsen	Brooks, Alta.
2003	Marie-Eve Pilon	Brownsburg-Chatham, Que.
2004	Nathalie Richard	Halifax, N.S.
2005	Joan Hoskinson	Thunder Bay, Ont.
2006	Nathalie Richard	St-Jean-Sur-Richelieu, Que.
2007	Nathalie Richard	St-Jean-Sur-Richelieu, Que.
2008	Nathalie Richard	St-Jean-Sur-Richelieu, Que.
2009	Nathalie Richard	St-Jean-Sur-Richelieu, Que.
2010	Nathalie Richard	St-Jean-Sur-Richelieu, Que.
2011	Nathalie Richard	Halifax, N.S.
2012	Nathalie Richard	Halifax, N.S.
2013	Lyne Murphy	St. Constant, Que.

25.19 Canadian Rally Novice Drivers' Championship for the Karel Pesek Memorial Trophy

25.19.1 This championship was inaugurated in 1975 to encourage new drivers to compete in the Canadian championship events. Eligibility and scoring are as follows:

- (a) Eligibility is for novice first drivers as defined in NRR 11.1.3 and holding Canadian rally licences.

- (b) Championship points shall be awarded eligible first drivers as in NRR 25.6.2 above.

25.19.2 Previous winners:

1975	Adrian Carson	Winnipeg, Man.
1975	Allan Cook	Ottawa, Ont.
1976	Mario Collin	Montreal, Que.
1977	Jocelyn Beaulieu	Montreal, Que.
1978	Karel Pesek	Toronto, Ont.
1979	Niall Leslie	Mississauga, Ont.
1980	Gerard Seguin	St. Lin, Que.
1981	Kelvin Lomheim	Calgary, Alta.
1982	Shawn Bishop	Calgary, Alta.
1983	Jacques Besner	Lorraine, Que.
1984	Francois Arbique	Montreal, Que.
1985	Don MacDougall	Edmonton, Alta.
1986	Andy Browning	Brampton, Ont.
1987	Tim O'Neil	Whitefield, NH
1988	Joseph Sears	Fairvale, NB
1989	Kent Atherton	Westfield, NB
1989	Jacques L'Estage	St-Jacques-le-Mineur, Que.
1990	Frank Sprongl	Woodbridge, Ont.
1991	Yoshio Kenoshita	Toronto, Ont.
1992	Lesley Ann Sudard	Wilmington, DE
1993	Jean-Sebastien Besner	Montreal, Que.
1994	Jean-Philippe Chevarier	St. Bruno, Que.
1995	Pete Pollard	Etobicoke, Ont.
1996	Chris Havas	Richmond, VT
1997	Owen Cule	Scarborough, Ont.
1998	Steve Barnes	Calgary, Alta.
1999	Bruno Laverdiere	Laval, Que.
2000	Brian Scott	Lake Orion, MI
2001	Antoine L'Estage	L'Acadie, Que.
2002	Andrew Comrie-Picard	Edmonton, Alta.
2003	Mathieu L'Estage	St.-Paul-de-l'Île-aux-Noix, Que.
2004	Norm LeBlanc	Pemberton, B.C.
2005	Robert MacKenzie	Bond Head, Ont.
2006	Maxime Labrie	La Prairie, Que.
2007	Craig Henderson	St-Constant, Que.
2008	Hardy Schmidtke	Cochrane, Alta.
2009	Nick Mathew	Mississauga, Ont.
2010	Ugo Desgreniers	Orford, Que.
2011	Maxwell Riddle	Vancouver, B.C.
2012	Alexandre Beland	Val d'Or, Que
2013	Jeremy Norris	Kitchener Ont.

25.20 Canadian Rally of the Year

- 25.20.1 The Canadian Rally of the Year award was presented as the Ken Vaughan Memorial Trophy from 1986 to 1990. The Ken Vaughan Memorial Trophy is now awarded as the CARS Board's Special Honours Award (see 25.21 below).

25.20.2 Previous winners:

1986	New Brunswick Lobster Rally	Ken Vaughan
1987	Rally of the Tall Pines	Ross Wood
1988	Dartmouth Highlands Rally	Clarke Paynter
1989	Rally of the Voyageurs	Linda & Terry Epp
1990	Rocky Mountain Rally	Lynn Nixon

25.21 The CARS Board's Special Honours Award for the Ken Vaughan Memorial Trophy

- 25.21.1 The Ken Vaughan Memorial Trophy is presented, from time to time, to honour those individuals who have contributed to the sport of rally over many years.

25.21.2 Previous winners:

1989	Jean Chevarier	St. Bruno, Que.
1990	Terry Epp	Stouffville, Ont.
1991	Ross Wood	Milton, Ont.
1992	John & Lynn Nixon	Calgary, Alta.
1994	Iain Tugwell	Clarence, NY
1995	Fred Baker	Scarborough, Ont.
1996	Linda Epp	Stouffville, Ont.
1997	Tom Burgess	Atlanta, GA

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1998	Dennis Wharton	Petersburg, Ont.
1999	not awarded	
2000	George Dewar	Riverview, NB
2001	Jean-George Marcotte	St-Anne Bellevue, Que.
2002	Don MacDougall	Edmonton, AB
2003	Clarke Paynter	Dartmouth, N.S.
2004	Denyse Moisan	Bois des Filion, Que.
2005	not awarded	
2006	not awarded	
2007	not awarded	
2008	Suzanne Barry	Calgary, Alta.
	Shawn Bishop	Calgary, Alta.
2009	not awarded	
2010	not awarded	
2011	Paul Westwick	Vancouver, BC
2012	not awarded	
2013	B.Gilles Lacroix	Mirabel, Que.

26. NORTH AMERICAN RALLY CUP**26.1 The North American Rally Cup for Drivers and Co-Drivers**

The NARC cup is a competition designed to stimulate international competition between Canada and the United States. Events counting for this cup are all of the full national championship events in the CARS Canadian Rally Championship and RA's Rally America Championship. The rally regulations governing each championship and series shall apply to the respective events. Competitors shall be properly licenced and scoring is as follows.

26.1.1 The driver and co-driver shall receive equal points.

26.1.2 Competitors may score their six best finishing positions between January first and December thirty-first. However, no more than four (4) events may be counted from either country. To be eligible to qualify for points in the North American Rally Cup, a competitor must have started a minimum of two (2) events in each country.

26.1.3 Cup points shall be awarded according to the following schedule:

First	20 pts.	Sixth	6 pts.
Second	15 pts.	Seventh	4 pts.
Third	12 pts.	Eighth	3 pts.
Fourth	10 pts.	Ninth	2 pts.
Fifth	8 pts.	Tenth	1 pt.

26.1.4 In the case of a tie for the first position in the drivers or co-drivers cup, the total points earned shall be used to determine their position, the higher number of points earned taking the winning position.

26.1.5 Points earned in one category, driver versus co-driver, shall not be transferable to the other category.

26.1.6 Competitors shall not switch duties in the vehicle from that stated on the official entry form once the competition has begun so as to unfairly earn points in the other category.

26.1.7 Awards will be presented at the AGM of the winner's ASN.

26.1.8 Previous winners:

Driver:

1976	John Buffum	Vermont
1977	John Buffum	Vermont
1978	John Buffum	Vermont
1979	Taisto Heinonen	British Columbia
1980	John Buffum	Vermont
1981	Taisto Heinonen	British Columbia
1982	Taisto Heinonen	British Columbia
1983	John Buffum	Vermont
1984	John Buffum	Vermont
1985	John Buffum	Vermont
1986	John Buffum	Vermont
1987	John Buffum	Vermont
1988	Paul Choiniere	Vermont
1989	Paul Choiniere	Vermont
1990	Bruno Kreibich	New York
1991	Bruno Kreibich	New York
1992	Paul Choiniere	Vermont
1993	Carl Merrill	Maine
1994	Frank Sprongl	Ontario
1995	Frank Sprongl	Ontario
1996	Carl Merrill	Maine
1997	Frank Sprongl	Ontario
1998	Carl Merrill	Maine
1999	Frank Sprongl	Ontario
2000	Karl Scheible	New York
2001	Tom McGeer	Ontario
2002	Tom McGeer	Ontario
2003	Tom McGeer	Ontario
2004	Patrick Richard	British Columbia
2005	Matthew Iorio	New Hampshire
2006	Matthew Iorio	New Hampshire
2007	Antoine L'Estage	Quebec
2008	Antoine L'Estage	Quebec
2009	Andrew Comrie-Picard	Ontario
2010	Antoine L'Estage	Quebec
2011	Antoine L'Estage	Quebec

2012	Antoine L'Estage	Quebec
2013	Antoine L'Estage	Quebec

Co-Driver:

1976	John Bellefleur	Ontario
1977	Vicki	Vermont
1978	Doug Shepherd	Michigan
1979	Tom Burgess	British Columbia
1980	Doug Shepherd	Michigan
1981	Tom Burgess	British Columbia
1982	Doug Shepherd	Michigan
1983	Doug Shepherd	Michigan
1984	Tom Grimshaw	Texas
1985	Tom Grimshaw	Kansas
1986	Tom Grimshaw	Kansas
1987	Tom Grimshaw	Kansas
1988	Clark Bond	Michigan
1989	Joe Andrieni	Michigan
1990	Jeff Becker	New York
1991	Jeff Becker	New York
1992	Jeff Becker	New York
1993	Jeff Becker	New York
1994	Jeff Becker	New York
1995	Jeff Becker	New York
1996	John Bellefleur	Ontario
1997	Dan Sprongl	Ontario
1998	Lance Smith	Vermont
1999	Dan Sprongl	Ontario
2000	Mark Williams	Maryland
2001	Mark Williams	Maryland
2002	Mark Williams	Maryland
2003	Jeff Becker	New York
2004	Nathalie Richard	Nova Scotia
2005	Nathalie Richard	Quebec
2006	Ole Holter	California
2007	Nathalie Richard	Quebec
2008	Nathalie Richard	Quebec
2009	Nathalie Richard	Quebec
2010	Nathalie Richard	Quebec
2011	Nathalie Richard	Nova Scotia
2012	Nathalie Richard	Nova Scotia
2013	Nathalie Richard	Nova Scotia

26.2 The North American Rally Cup for Marques

The NARC cup for Marques is designed to stimulate interest in and support of stage rallies by manufacturers.

- 26.2.1 If the winning make of automobile is not manufactured in North America, the cup will be received by the appointed distributor or agent.
- 26.2.2 A marque will receive the number of points earned by the highest placing automobile of that make per the schedule in NRR 26.1.3 above, in all events qualifying per NRR 26.1 above.
- 26.2.3 The vehicle need not be the same model or year to be considered the same marque (i.e.: the Cup will determine the make of automobile winning the most points such as Ford versus Chevrolet, not Ford Capri versus Ford Escort, etc.)
- 26.2.4 Presentation of the award will be made to the manufacturer or appointed agent or distributor in the country where the majority of points were earned at that country's ASNs annual awards ceremony.
- 26.2.5 Previous winners:

1976	Datsun	1996	Eagle
1977	Datsun	1997	Audi
1978	Datsun	1998	Audi
1979	Datsun	1999	Audi
1980	Triumph	2000	Subaru
1981	Datsun	2001	Subaru
1982	Datsun	2002	Subaru
1983	Audi	2003	Mitsubishi
1984	Audi	2004	Subaru

1985	Toyota	2005	Subaru
1986	Audi	2006	Subaru
1987	Audi	2007	Subaru
1988	Audi	2008	Subaru
1989	Mazda	2009	Subaru
1990	Audi	2010	Mitsubishi
1991	Audi	2011	Subaru
1992	Audi	2012	Mitsubishi & Subaru (tie)
1993	Audi	2013	Subaru
1994	Audi		
1995	Audi		

26.3 North American Rally Cup for Production Class

26.3.1 From 1990 to 2008, there was a championship for production Drivers, Co-drivers and Marques. Eligible vehicles were those which meet the Production Class rules of either the CARS Rally Regulations or the RA or NASA Rally Rules in Production and Production GT. Prior to 1989, there was only one production class where eligible vehicles were grouped together.

26.3.2 Previous winners:

Driver:

1986	Nelson Shepard	Vermont	
1987	Niall Leslie	Ontario	
1988	Dan Gilliland	Michigan	
1989	Tim O'Neil	New Hampshire	(Prod)
	Doug Shepherd	Michigan	(Prod GT)
1990	Mark Molnar	Ohio	(Prod)
	Alan Freed	Virginia	(Prod GT)
1991	Sylvain Vincent	Quebec	(Prod)
	Jim Wilson	New York	(Prod GT)
1992	Barry Latreille	Ontario	(Prod)
	not awarded		(Prod GT)
1993	Barry Latreille	Ontario	(Prod)
	Tom McGeer	Ontario	(Prod GT)
1994	Walt Petersen	Alberta	(Prod)
	Selcuk Karamanoglu	Maine	(Prod GT)
1995	Bob Elliott	Vermont	(Prod)
	Selcuk Karamanoglu	Maine	(Prod GT)
1996	Peter Watt	Ontario	(Prod)
	Selcuk Karamanoglu	Maine	(Prod GT)
1997	Peter Watt	Ontario	(Prod)
	Bill Driegert	Texas	(Prod GT)
1998	Jay Kowalik	Ohio	(Prod)
	not awarded		(Prod GT)
1999	Karl Scheible	New York	(Prod)
	Lee Shadbolt	Orgeon	(Prod GT)
2000	Ted Mendham	New Hampshire	(Prod)
	Patrick Richard	British Columbia	(Prod GT)
2001	Peter Reilly	Ontario	(Prod)
	Patrick Richard	British Columbia	(Prod GT)
2002	Ted Mendham	New Hampshire	(Prod)
	Julien Pilon	Quebec	(Prod GT)
2003	Vittorio Bares	New Hampshire	(Prod)
	Julien Pilon	Quebec	(Prod GT)
2004	Scott Comens	Ontario	(Prod)
	Joan Hoskinson	Ontario	(Prod GT)
2005	Joan Hoskinson	Ontario	(Prod GT)
2006	Jim Stevens	Ontario	(Prod)
	Norm LeBlanc	British Columbia	(Prod GT)
2007	Jan Zedril	Manitoba	(Prod)
	Ted Mendham	New Hampshire	(Prod GT)
2008	Jan Zedril	Manitoba	(Prod)
	Not Awarded		(Prod GT)

Co-Driver:

1986	Diane Houseal	Pennsylvania	
1987	Betty-Ann Gilliland	Michigan	
1988	Betty-Ann Gilliland	Michigan	
1989	Martin Headland	Ontario	(Prod)
	Joe Andrieni	Michigan	(Prod GT)
1990	Yorgi Bittner	Ohio	(Prod)
	Jim Kloosterman	South Carolina	(Prod GT)

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1991	Paul Turgeon	Quebec	(Prod)
	James Wilson	New York	(Prod GT)
1992	Sandra Latreille	Ontario	(Prod)
	not awarded		(Prod GT)
1993	Sandra Latreille	Ontario	(Prod)
	Trish Sparrow	Ontario	(Prod GT)
1994	Harry Pressey	Ontario	(Prod)
	John McArthur	New York	(Prod GT)
1995	Dick Casey	Virginia	(Prod)
	Constantine Mantopoulos	New York	(Prod GT)
1996	Dick Casey	Virginia	(Prod)
	Yorgi Bittner	Ohio	(Prod GT)
1997	Dick Casey	Virginia	(Prod)
	Hi Shin Shim	Texas	(Prod GT)
1998	not awarded		(Prod)
	not awarded		(Prod GT)
1999	Gail McGuire	New York	(Prod)
	Claire Chizma	California	(Prod GT)
2000	Lise Mendham	New Hampshire	(Prod)
	John McLeod	Massachusetts	(Prod GT)
2001	Ray Felice	Ontario	(Prod)
	Ian McCurdy	British Columbia	(Prod GT)
2002	Lise Mendham	New Hampshire	(Prod)
	Marie-Eve Pilon	Quebec	(Prod GT)
2003	Forest Fyberg	New Hampshire	(Prod)
	Marie-Eve Pilon	Quebec	(Prod GT)
2004	Adrian Wintle	Ontario	(Prod)
2005	Jeff Secor	Michigan	(Prod GT)
2006	not awarded		(Prod)
	Keith Morison	Alberta	(Prod GT)
2007	Jody Zedril	Manitoba	(Prod)
	Lyse Mendham	New Hampshire	(Prod GT)
2008	Jody Zedril	Manitoba	(Prod)
	Not Awarded		(Prod GT)

Marque:

1986	Dodge	2001	Volkswagen	(Prod)
1987	Volkswagen		Subaru	(Prod GT)
1988	Dodge	2002	Volkswagen	(Prod)
1989	Volkswagen (Prod)		Subaru	(Prod GT)
	Mazda (Prod GT)	2003	Volkswagen	(Prod)
1990	Volkswagen (Prod)		Subaru	(Prod GT)
	Mazda (Prod GT)	2004	Volkswagen	(Prod)
1991	Volkswagen (Prod)		Subaru	(Prod GT)
	Mazda (Prod GT)	2005	Suzuki	(Prod)
1992	Volkswagen (Prod)		Subaru	(Prod GT)
	Subaru (Prod GT)	2006	Volkswagen	(Prod)
1993	Ford (Prod)		Subaru	(Prod GT)
	Subaru (Prod GT)	2007	Honda	(Prod)
1994	Honda (Prod)		Subaru	(Prod GT)
	Mazda (Prod GT)	2008	Mitsubishi	(Prod)
1995	Volkswagen (Prod)		Subaru	(Prod GT)
	Eagle (Prod GT)			
1996	Suzuki (Prod)			
	Eagle (Prod GT)			
1997	Honda (Prod)			
	Eagle (Prod GT)			
1998	Neon (Prod)			
	Eagle (Prod GT)			
1999	Volkswagen (Prod)			
	Subaru (Prod GT)			
2000	Volkswagen (Prod)			
	Subaru (Prod GT)			

26.4 North American Rally Cup for 2WD

26.4.1 The NARC cup for 2WD is designed to stimulate interest a combined championship for Drivers, Co-Drivers and Marques competing in 2WD vehicles.

26.4.2 From 1986 through 1989 there was a championship for FIA Group A class for Drivers, Co-Drivers and Marques. From 1993 to 2000, the championship added an Open Class for Drivers, Co-Drivers and Marques for all competitors competing in vehicles which did not fit into the production class championships. From 2001 to 2007, the championship was for Driver, Co-Drivers and Marques competing in vehicles which met Group N specifications. In 2009, the championship was opened to all 2WD vehicles.

26.4.3 Scoring for the North American Rally Cup for 2WD will be per the scoring rules of NRR 26.1 & 26.2.

26.4.4 Presentation of awards will be per NRR 26.1 and 26.2.

26.4.5 Previous winners:

Driver:

1986	Walter Boyce	Ontario
1987	Jean-Paul Perusse	Quebec
1988	Paul Choiniere	Vermont
1989	Demetrious Andreou	Ontario
1993	Carl Merrill	Maine
1994	Frank Sprongl	Ontario
1995	Carl Merrill	Maine
1996	Carl Merrill	Maine
1997	Frank Sprongl	Ontario
1998	Carl Merrill	Maine
1999	Frank Sprongl	Ontario
2000	Karl Scheible	New York
2001	not awarded	
2002	Patrick Richard	British Columbia
2003	Peter Thomson	Ontario
2004	Not awarded	
2005	Not awarded	
2006	Not awarded	
2007	Not awarded	
2008	Not awarded	
2009	Zbigniew Szewczyk	Alberta
2010	Grant Riddell	Ontario
2011	Not awarded	
2012	Edward McNelly	New Hampshire
2013	Not awarded	

Co-Driver:

1986	Terry Epp	Ontario
1987	Martin Headland	Ontario
1988	Martin Headland	Ontario
1989	Dave Gill	Ontario
1993	Jeff Becker	New York
1994	Jeff Becker	New York
1995	Jeff Becker	New York
1996	John Bellefluer	Ontario
1997	Dan Sprongl	Ontario
1998	Lance Smith	Vermont
1999	Dan Sprongl	Ontario
2000	Mark Williams	Maryland
2001	Not awarded	
2002	Not awarded	
2003	Not awarded	
2004	Not awarded	
2005	Not awarded	
2006	Not awarded	
2007	Not awarded	
2008	Not awarded	
2009	Tomasz Karzynski	Alberta
2010	Not awarded	
2011	Not awarded	
2012	Ole Holter	California
2013	Not awarded	

Marque:

1986	Volkswagen	2003	Subaru
1987	Volkswagen	2004	Subaru
1988	Audi	2005	Subaru
1989	Mazda	2006	Not awarded
		2007	Not awarded
1993	Audi	2008	Volkswagen
1994	Audi	2009	Ford, Volkswagen (tie)
1995	Ford	2010	Volkswagen
1996	Eagle	2011	Volkswagen
1997	Audi	2012	Ford
1998	Audi	2013	Ford

1999 Audi
 2000 Subaru
 2001 Mitsubishi
 2002 Subaru

27. CANADIAN PERFORMANCE RALLY LIFETIME STANDINGS

The following standings, compiled by George Dewar, are based on an individual's placings in the first six overall positions (worth 100, 80, 60, 50, 40, and 30 points respectively) on all Canadian Championship Performance Rallies from 1973 to present.

Figures in the first column represent the Lifetime points; figures in the second column represent the total CARS championship points earned; and figures in the third column represent the number of 1st overall event placings, or wins.

GRAND MASTER (>2000 points)								
Driver	Lifetime	CARS	Wins	Co-Driver	Lifetime	CARS	Wins	
1 Taisto Heinonen	5580	1104	40	1 Tom Burgess	6080	1211	39	
2 Frank Sprongl	5000	1064	36					
3 Tom McGeer	4990	1053	21					
4 John Buffum	4900	778	43					
5 Antoine L'Estage	4590	1012	33					
				2 Dan Sprongl	4520	966	32	
6 Patrick Richard	4080	877	22	3 John Bellefleur	3930	765	20	
				4 Nathalie Richard	3610	806	29	
7 Jean-Paul Perusse	3860	784	21					
8 Walter Boyce	2920	593	12	5 Trish McGeer	2860	636	5	
9 Randy Black	2880	570	10					
10 Bo Skowronnek	2390	503	5	6 Martin Headland	2250	495	7	
11 Carl Merrill	2350	382	8	7 BGilles Lacroix	2140	561	3	
12 Sylvain Vincent	2140	477	0					
13 Bjorn Anderson	2120	420	6	8 Mark Williams	2050	361	17	
MASTER (1000 - 1990 points)								
Driver	Lifetime	CARS	Wins	Co-Driver	Lifetime	CARS	Wins	
				9 Brian Maxwell	1920	484	5	
				10 Robin Edwardes	1870	389	3	
				11 Sandra Latreille	1780	420	1	
14 Jonathan Nichols	1760	402	0	12 Alan Ockwell	1740	406	10	
				13 Louis Belanger	1730	372	3	
				14 Dominique Cyr	1700	359	0	
15 Tim Bendle	1690	370	11					
16 Yves Barbe	1660	385	3					
17 Sylvain Erickson	1650	352	5					
18 Doug Leverton	1640	335	0					
19 Barry Latreille	1630	390	0					
20 Niall Leslie	1600	366	0					
21 Andrew Comrie-Picard	1540	330	1					
				15 Ken Humphrey	1520	317	0	
				16 Terry Epp	1460	328	3	
				17 Doug Woods	1450	290	11	
				18 Jeff Becker	1370	147	8	
22 Paul Choiniere	1320	174	8					
				19 Brian Jackson	1310	262	5	
				20 Vicki	1290	256	10	
				21 Ian McCurdy	1260	262	5	
				22 Mike Koch	1220	291	0	
23 Bernard Franke	1180	248	3	23 Richard Cyr	1180	247	3	
24 Alain Bergeron	1170	235	3					
				24 Rod Hendricksen	1140	228	2	
25 Bruno Kreibich	1100	30	5					
				25 Tom Grimshaw	1060	144	8	
				26 Lynn Nixon	1040	229	3	
				27 Philip Erickson	1040	213	4	
				28 Walt Leemans	1030	209	2	
				29 Bob Lee	1020	216	3	
26 Jorge Dascollas	1010	233	1					
				30 Yanick Napert	1000	232	3	

EXPERT (500 - 990 points)

Driver	Lifetime	CARS	Wins	Co-Driver	Lifetime	CARS	Wins
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27 John Nixon	990	223	1	31 Doug Shepherd	980	179	7
28 Leo Ulrichich	970	239	1	32 Suzanne Stewart	970	263	1
29 Andy Falkiewicz	970	224	0	33 Mark Goldfarb	970	220	1
30 Jean-Sebastien Besner	960	222	0	34 Lyne Murphy	960	262	
31 Peter Thomson	940	215	2	35 Clarke Paynter	910	234	0
32 Craig Henderson	920	236		36 Lance Smith	910	139	3
33 Andre Normandin	890	201	1				
34 Lauri Paivarinta	890	189	3	37 Angus Chisholm	830	184	0
35 Shawn Bishop	870	241	1	38 Raymond Cadieux	810	169	2
36 John Lutzenberger	840	172	3	39 Steve Walkington	800	185	1
37 Gord Olsen	790	214	0				
38 Rod Millen	780	154	5				
39 Bruno Carre	770	213	1				
40 Wojtek Grabowski	770	174	2				
41 Tim O'Neil	760	144	2				
				40 Yvan Joyal	750	221	1
				41 Francois Cyr	750	160	1
				40 Yvan Joyal	750	221	1
				41 Francois Cyr	750	160	1
42 Clive Smith	740	142	2	42 Jacques Besner	740	168	0
				43 Rod MacDonald	730	152	3
43 John Paynter	720	180	0	44 Jon Wickens	720	55	5
44 Michel Poirier-Defoy	710	181	2				
45 Selcuk Karamanoglu	670	100	2	45 Mary Crundwell	670	152	3
				46 Lorne Nicklason	670	133	2
46 Mathew Iorio	660	135	2				
47 Philippe Dube	650	184	1				
				47 Ole Holter	630	145	2
				48 Tomasz Karzynski	610	164	0
				49 Lee Bartholomew	610	124	3
48 Guy Vanier	600	115	2	50 Todd Patola	600	164	0
49 Keith Townsend	590	146	0				
				51 Marcel-Paul Raymond	580	140	0
50 Simon Losier	570	193		52 Paul Manson	570	119	2
51 Sven Halle	570	111	0	53 David Shindle	570	106	0
				54 Doug Ort	560	114	1
52 Chris Castledine	510	116	0	55 Robert Labrie	510	155	0
53 Jeff Vanderwall	510	102	0				
SENIOR (250 - 490 points)							
Driver	Lifetime	CARS	Wins	Co-Driver	Lifetime	CARS	Wins
54 Nelson Shepherd	490	110	0	56 Art MacKenzie	490	102	3
55 Eric Jones	490	104	0	57 John Buffum	490	61	3
				58 Keith Morison	480	138	0
				59 Gilles Nault	480	111	1
				60 Eric Tremblay	460	124	0
				61 Ron Arthur	460	106	0
56 Hardy Schmidtke	440	160		62 Fred Baker	440	90	0
57 Joel Levac	440	131	0	63 Gilles Vanier	440	90	2
58 Glen Thomas	420	93	0	64 Ross Wood	420	112	0
				65 Carl Schenk	420	106	0
				66 Howard Davies	420	0	2
59 John Medwell	410	84	2				
60 Walter Petersen	400	123	0	67 Paul Choiniere	400	60	4
61 Gordon Lansdell	400	93	0				
62 Norm LeBlanc	390	94	0	68 Gary Murakami	390	109	0
				69 Marie-Therese Rousseau	380	80	1
63 Dan Sabourin	370	74	0				
64 Mathieu L'Estage	360	113	0	70 Catherine Skowronnek	360	83	2
65 Thierry Menegoz	360	85	1	71 Martin Brady	360	82	1
				72 Tony Woodlands	360	82	0
				73 Clarke Bond	360	13	1
66 Steeve Hobbs	350	110		74 Jean-Mathieu Tremblay	350	110	
67 Jean-Marc Alcaraz	340	77	0	75 Rob Fagg	340	74	1
68 Ari Montonen	330	103	0				
69 Gary Neil	330	77	0				
70 Tim Mullin	330	69	0				
71 Bob Garside	320	77	0	76 Rick Sakanashi	320	79	0
72 Francois Arbiqque	320	74	0	77 Yurek Dabrowski	320	73	1
73 Julien Pilon	320	70	0	78 Marie-Eve Pilon	320	73	0
				79 Yorgi Bittner	320	64	2
74 Max Riddle	310	95		80 Ian McEwen	310	66	0
75 Paul Kankainen	310	78	0	81 Paul Kluckner	310	62	0

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76	Ugo Desgreniers	300	95	82	John Hall	300	116	
77	Jacques Besner	300	85	0	83	Erik Kirby	300	95
78	Murray Muncie	300	79	0	84	Chloe Erickson	300	80
79	Bryson Guptill	300	73	0	85	David Watts	300	69
80	Brian Scott	300	71	0	86	Larry Dea	300	59
81	Maxime Labrie	290	94		87	Steve Farrell	290	97
82	Elizabeth Campbell	290	89	0	88	Don Ford	290	55
83	Chuck MacLaren	290	55	0	89	Scott Weinheimer	290	22
84	Rejean Losier	280	109	0	90	Betty-Ann Gilliland	280	75
85	Dan Gilliland	280	71	0	91	Craig Parry	280	61
86	Brent Redford	280	65	0	92	Greg Hofer	280	57
87	Tom Novak	280	59	0	93	Grant Whittaker	280	55
88	Jeff Smith	270	61	0	94	Helen Bell	270	57
89	Frederic Labrie	260	76	0	95	Paul Turgeon	260	70
90	Martin Gauvin	260	67	0	96	Dan Duchesneau	260	67
					97	Bob Burton	260	61
					98	Carl Williamson	260	61
					99	Robert Thibault	260	56
91	Janusz Komorowski	250	56	0	100	Malcolm Swan	250	77
92	Ross Sutcliffe	250	48	0	101	Art Losier	250	70
					102	Pierre Racine	250	64

There are 9 other drivers and 22 other co-drivers with at least one national win who do not yet have senior status.

The following are general rules for all Rally Sprints run by CARS affiliated clubs. Additional rules may be added by an individual region and/or a Rally Sprint series to supplement these regulations, but these regional or series regulations may not contravene these National Rally Sprint Regulations.

28. DEFINITION:

Rally Sprints are competition events where participants compete, one at a time, against the clock and total time taken over the competitive sections of the event determines placing. Rally Sprints are both a level of competitive event on their own and a development event preparing competitors for participation in performance rally events. Rally Sprints are meant to be designed to accent the development of vehicle handling skills while limiting both top and average speeds attainable by competitors. Rally Sprints are of two basic types: (a) single course at a single venue or multiple courses at a single venue or multiple venues (hereafter called Rally Cross) or (b) multiple courses at either a single venue or multiple venues which could include closed public roads (hereafter called Rally Sprint). Both Rally Cross and Rally Sprints could follow a rally format of stage and transit

28.1 Rally Cross

28.1.1 General Requirements

- 28.1.1.1 A Rally Cross is an event generally held on an unpaved, flat surface, wherein the course generally consists of straight sections and connecting turns or corners. Accent in course design is also placed on the general avoidance of obstacles such as trees, exposures, deep ditches, etc.
- 28.1.1.2 Only one car is permitted on the course at any one time.
- 28.1.1.3 Rally Cross events are to be insured under CARS Rally Cross insurance, available through CARS. The event insurance certificate must be displayed at the event registration.
- 28.1.1.4 All persons allowed on site, must sign the insurance waiver.
- 28.1.1.5 Organizers reserve the right to exclude any vehicle or person from further participation in the event at any time for reasons of perceived safety concerns involving either the condition of the vehicle or the conduct of the person.

28.1.2 Eligibility of Competitors and Vehicles

28.1.2.1 Competitors

- (a) The following documents, valid on the day(s) of the event, must be presented at registration:
 - driver's licence for the crew member listed as driver
 - vehicle registration (if road licenced vehicle)
 - when the owner of the vehicle is not entered as the driver or passenger, a written authorization from the owner of the vehicle
 - for any participant (driver or passenger) under the age of majority of the province, a written permission signed by a parent or guardian
- (b) Passengers (sixteen or older) are permitted.
- (c) If the Rally Cross is being presented as a stage and transit format event, then a co-driver must be carried. No other persons are permitted in the vehicle.
- (d) If the RallyCross is being presented as a stage and transit format event, then the vehicle must be a street licenced and insured vehicle.

28.1.2.2 Vehicles

- (a) Vehicles must be based on a production, closed body and chassis to compete.
- (b) All loose items in the vehicle must be removed during the event (including from the trunk).
- (c) Snap-on hubcaps, detachable fender skirts and trim rings must be removed.
- (d) Tires must be in good condition, with no cord or belts showing or cracks in the tread or sidewalls.
- (e) Mechanical condition and safety of the vehicle is the responsibility of the competitor.

28.1.2.3 Mandatory equipment

- (a) Helmets meeting NRR.11.1.6 or bearing "Snell Memorial Foundation" 1995 M or 1995 SA (or more recent) labels must be worn by all drivers and passengers. A DOT motor vehicle helmet may be worn if it is the type that covers the ears (no skull caps).
- (b) Minimum three point seat harnesses must be worn by drivers and passengers.

28.1.3 Event Organization

28.1.3.1 Authorizations and Permits

Proper authority for use of the venue(s) must be obtained and such permissions must be available for scrutiny at registration.

28.1.3.2 Security Measures

Security measures will be in force to insure the safety of competitors, officials and spectators (closing of the course, start and finish locations, communications, recovery, etc.)

28.1.3.3 Course Design

- (a) Each course shall not exceed 3 km long.
- (b) The course shall be arranged such that the maximum speed for an average competitor shall not exceed 70 km/h.
- (c) The course shall be arranged such that it is clear of all heavy stationary objects. If this is not possible, tires, hay bales or other compressible barrier shall be used to minimise vehicle damage should contact occur.
- (d) The course should be configured in such a way as to minimize the possibility of rollover.
- (e) The use of chicanes (consisting of hay bales, pylons or similar soft materials) on straights of greater than 100 m is recommended. The use of bumps or yumps to reduce speeds is not permitted.
- (f) Organizers reserve the right to change the course configuration at any time during the competition should a portion of the course deteriorate.
- (g) Marshals shall be placed such that all competing vehicles will be visible at all times while on the route.
- (h) A stop box may be used at the end of a run. A time penalty of 10 seconds must be assessed if a vehicle does not stop within the box.
- (i) Each driver will be provided an opportunity to walk or drive through the course, or to have a parade lap, prior to the first run.

28.2 Rally Sprint

28.2.1 General Requirements

28.2.1.1 A Rally Sprint is an event generally held on private or controlled public roads and is made up of short stages (closed to public traffic) and transits (open to public traffic). Performance is measured by timing procedures as defined in CARS national rally regulations (see NRR 15 and 16).

28.2.1.2 Only one car is permitted on a stage at any one time although several stages may be running simultaneously.

28.2.1.3 Rally Sprint events are to be insured under CARS rally sprint insurance available through the CARS. The event insurance certificate must be displayed at the event registration.

28.2.1.4 All persons involved in the event as participants, officials, service crews, etc. must sign the insurance waiver.

28.2.1.5 Organizers reserve the right to exclude any vehicle or participant from further participation in the event at any time for reasons of perceived safety concerns involving either the condition of the vehicle or the conduct of the participant.

28.2.2 Eligibility of Competitors and Vehicles

28.2.2.1 Competitors

The following documents, valid on the day(s) of the event, must be presented at registration:

- (a) driver's licence for the crew member listed as driver
- (b) vehicle registration
- (c) liability insurance for the entered vehicle
- (d) when the owner of the vehicle is not entered as the driver or passenger, a written authorization from the owner of the vehicle
- (e) for any participant (driver or passenger) under the age of majority of the province, a written permission signed by a parent or guardian
- (f) rally sprint competition licences, issued by the CARS region, for both driver and co-driver (regional or national licences also accepted)

28.2.2.2 Vehicles

- (a) Vehicles must be based on a production, closed body and chassis to compete.

- (b) All competing vehicles must be roadworthy and the following items, in particular, must be adequate and functioning properly: all brakes, horn, windshield wipers, all legally required exterior lights, tires (including all spares) and exhaust system.
- (c) All loose items in the vehicle must be fastened securely during the event (including items in the trunk).
- (d) Mechanical condition and safety of the vehicle is the responsibility of the competitor.

28.2.2.3 Mandatory equipment

- (a) Helmets conforming to NRR 11.1.6 must be worn by all drivers and co-drivers.
- (b) A tow rope must be carried in the vehicle.
- (c) Safety equipment conforming to NRR 12.3.2 to 12.3.9 (Roll cage, harnesses, etc.)
- (d) Driving suits conforming to NRR 11.1.8 are recommended.

28.2.3 Event Organization

28.2.3.1 Authorizations and Permits

- (a) Proper authority for use of the venue must be obtained and such permissions must be available for scrutiny at registration.
- (b) The event must have a permit issued by a CARS region.

28.2.3.2 Security Measures

Security measures must be in force to insure the safety of competitors, officials and spectators (closing of roads, traffic controls, start and finish locations, communications, recovery, etc.)

28.2.3.3 Course Design

- (a) The special stages of the event will be run on short (maximum 3 km) segments of road, public or private, closed to all traffic.
- (b) The roads used as special stages shall be carefully chosen to reflect the overall intention of rally sprint events to be a performance rally entry level.
- (c) The roads used as special stages shall be chosen so that the average speed for the fastest competitor does not exceed 80 km/h. In case this condition is not met, despite best efforts, organizers will have to take action to correct this situation in order to use this road again in the future.
- (d) Any competitor, who exceeds an average speed of 80.0 km/h in a stage, shall be scored with a time equivalent to 80.0 km/h, rounded up to a whole second. All others shall be scored with the time taken.
- (e) In addition to (d) above, any stage in which any competitor exceeds an average speed of 80.0 km/h plus 10% shall not count in the scoring of the event.

28.2.3.4 Event Officials

- (a) Marshals shall be placed along the stage to ensure security, timing and the proper management of the event.
- (b) Radio communications will be used in such a way that, in case of emergency, competing vehicles can be stopped.
- (c) Event officials will act as judges of fact for the whole duration of the event.

28.2.3.5 Documents

- (a) Supplementary regulations shall be made available before the event.
- (b) Necessary documents such as a detailed route book and odometer check shall be supplied to all participating teams.

FOR REFERENCE ONLY NOT APPLICABLE FOR NEW VEHICLES

29. ROLLCAGES: JANUARY 1, 2000 TO DECEMBER 31, 2008

The following rollcage specifications are for reference purposes only for cars with log books issued between January 1, 2000 and December 31st. 2008.

Please see regulation NRR 12.3.2 for new rollcage requirements.

29.1 Roll Over Protection.

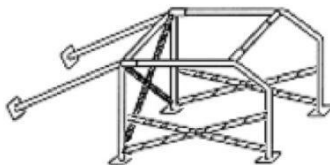
29.1.1 Rollcages are mandatory for all vehicles.

29.1.2 Specific roll over protection is subject to the approval of the scrutineer at each event.

29.1.3 Basic design considerations.

The basic purpose of the roll over protection is to prevent serious bodysell deformation, and so reduce the risk of injury to occupants, in the case of a collision or of a car turning over. The essential features of safety cages are sound construction designed to suit the particular vehicle, adequate mountings and a close fit to the bodysell. The safety cage must not unduly impede the entry or exit of the driver and co-driver.

29.1.4 The rollcage is a structural framework made up of a main rollbar and a front rollbar (or a main rollbar and two lateral rollbars), their connecting members, two diagonal members, backstays and mounting points. (For example, see diagram below.) Longitudinally, the safety cage must be entirely contained between the top mounting points of the front suspension and the top mounting points of the rear suspension.



29.1.5 Definitions:

(a) Main rollbar:

Structure consisting of a near-vertical frame or hoop located across the vehicle just behind the front seats. (see drawing 253-39)

(b) Front rollbar:

Similar to main rollbar but its shape follows the windscreen pillars and top screen edge.

(c) Lateral rollbar:

Structure consisting of a near-vertical frame or hoop located along the right or left side of the vehicle.

The rear legs of a lateral rollbar must be just behind the front seats.

The front leg must be against the windscreen pillar and the door pillar such that it does not unduly impede the entry or exit of driver and co-driver. (see drawing 253-40)

A lateral half-rollbar is a lateral rollbar connected to a main rollbar which constitutes its rear leg. (see drawing 253-4).

(d) Longitudinal member:

Longitudinal tube which is not a part of the main, front or lateral rollbar but which links them.

(e) Diagonal member:

Transverse tube between a top corner of the main rollbar or upper end of a backstay and a lower mounting point on the other side of the rollbar or backstay.

(f) Framework reinforcement:

Reinforcing member fixed to the rollcage to improve its structural efficiency.

(g) Reinforcement plate:

Metal plate fixed to the bodyshell or chassis structure under a rollbar mounting foot to spread load into the structure.

(h) Mounting foot:

Plate welded to a rollbar tube to permit its bolting or welding to the bodyshell or chassis structure, usually onto a reinforcement plate.

(i) Removable members:

Structural members of a safety cage which are able to be removed.

(j) Back Stay:

A bracing member which connects the top of the main hoop to the body of the car behind the main hoop.

29.1.6 Basic safety cage:

Only rollcages must be used.

29.1.6.1 Compulsory diagonal members:

Different ways of fitting the compulsory diagonal members: see drawings 253-3 to 253-5. The combination of several members is permitted.

29.1.6.2 Optional reinforcing members:

Each type of reinforcement (drawings 253-6 to 253-17, 253-17A and 253-17C) may be used separately or combined with others.

Longitudinal rollcage extensions are allowed up to the level of the original suspension mounting points on the shell. There must not be direct connection between the top extension and the bottom extension.

29.1.7 Main, front and lateral rollbars:

These frames or hoops must be made in one piece without joints. Their construction must be smooth and even, without ripples or cracks. The vertical part of the main rollbar must be as straight as possible and as close as possible to the interior contour of the bodyshell.

The front leg of a front rollbar or of a lateral rollbar must be straight, or if it is not possible, must follow the windscreen pillars and have only one bend with its lower vertical part. Where a main rollbar forms the rear legs of a lateral rollbar (drawing 253-4), the connection to the lateral rollbar must be at roof level.

One continuous length of tubing must be used for each of the hoops with smooth continuous bends and no evidence of crimping or wall failure.

The tubing must be bent by a cold working process and the centreline bend radius must be at least 3 times the tube diameter. If the tubing is ovalised during bending, the ratio of minor to major diameter must be 0.9 or greater.

To achieve an efficient mounting to the bodyshell, the original interior trim may be modified around the safety cages and their mountings by cutting it away or by distorting it.

However, this modification does not permit the removal of complete parts of upholstery or trim unless permitted elsewhere in these regulations.

29.1.8 Protective Padding:

Where the occupants' bodies could come into contact with the safety cage, flame retardant padding must be provided for protection.

Where the occupants' crash helmets could come into contact with the safety cage, the padding must comply with FIA standard 8857-2001, type A (see technical list n°23 "Roll Cage Padding Homologated by the FIA").

29.1.9 Doorbars (for side protection):

One or more longitudinal members must be fitted at each side of the vehicle (see drawings 253-7, 253-8, 253-12, 253-17). They may be removable. The side protection must be as high as possible, but its upper attachment points must not be higher than half the total height of the door measured from its base. If these upper attachment points are located in front of or behind the door opening, this height limitation is also valid for the corresponding intersection of the strut and the door opening.

In the case of doorbars in the form of an "X" (cross-struts), It is recommended that the lower attachment points of the cross-struts be fixed directly onto the longitudinal member and that at least one part of the "X" be a single-piece bar.

29.1.10 Backstays:

These are compulsory and must be attached near the roof line and near the top outer bends of the main rollbar on both sides of the car. They must make an angle of at least 30° with the vertical, must run rearwards and be straight and as close as possible to the interior side panels of the bodyshell.

Their materials specification, diameter and thickness must be as defined in 29.1.13

Their mountings must be reinforced by plates. Each backstay should be secured by bolts having a cumulative section area at least two thirds of that recommended for each rollbar leg mounting in 29.1.14, and with identical reinforcement plates of at least 60 cm² area (see drawing 253-25).

A single bolt in double shear is permitted, provided it is of adequate section and strength (see drawing 253-26) and provided that a sleeve is welded into the backstay.

29.1.11 Diagonal members:

At least one diagonal member must be fitted. The fitting of two diagonal members is recommended for all cars, and mandatory for all cars with a log book issued after January 1, 2002. The diagonals may be in the main hoop or in the backstays. For vehicles with log books issued after January 1, 2007, the diagonals must be in the main hoop. The connection between the two members must be reinforced by a gusset.

Drawings 253-3 to 253-5 are examples of diagonal members and they must be straight, not curved.

The attachment points of the diagonal members must be so located that they cannot cause injuries. They may be made removable but must be in place during events. The lower end of the diagonal must join the main rollbar or backstay not further than 100 mm from the mounting foot. The upper end must join the main rollbar not further than 100 mm from the junction of the backstay joint, or the backstay not more than 100 mm from its junction with the main rollbar.

They must comply with the minimum specification set out in 29.1.13. Diagonal members fixed to the bodyshell must have reinforcement plates as defined in 29.1.14.

29.1.12 Roof bars:

Effective Jan 1 2007, all vehicles must have at least one roof reinforcement bar, as shown in drawings 253-9A to 9E. Vehicles with log books issued after January 1, 2007 must have two roof reinforcement bars, as shown in drawings 253-9A to 9C.

29.1.13 Material.

- 29.1.13.1 Highly recommended for all events and vehicle classes, but required for all FIA sanctioned championship events: cold drawn seamless or DOM tubing. Material must be unalloyed carbon steel containing a maximum of 0.3% of carbon and having a minimum tensile strength of 350N/mm². The maximum content of additives is 1% for manganese and 0.5% for other elements.

The size of tubing to be used shall be as follows:

Dimensions (mm)	Dimensions (in)	Use
45 x 2.5 or 50 x 2.0	1.75" x 0.095" or 1.98" x 0.08"	Main rollbar (drawing 253-39) or lateral rollbars, and their connections (drawing 253-40), according to construction.
38 x 2.5 or 40 x 2.0	1.5" x 0.095" or 1.58" x 0.08"	Lateral half-rollbars and other parts of the safety cage.

- 29.1.13.2 For all other CARS vehicle classes and all other events, safety cages may use Seamless or DOM mild steel tubing.

The size of tubing to be used shall be as follows:

Dimensions (mm)	Use
44 x 3 (1.75"x0.12")	All parts of the safety cage.

- 29.1.13.3 Note that these figures represent the minima allowed. In selecting the steel, attention must be paid to obtaining good elongation properties and adequate weld ability.

An inspection hole of at least 4.5 mm diameter must be drilled in a non-critical area of the main hoop to facilitate verification of wall thickness.

- 29.1.13.4 Where nuts and bolts are used, they must be of at least M8 size of ISO standard 8.8 or better.

Fasteners must be self-locking or fitted with lock washers.

- 29.1.14 Mounting of rollcages to the bodyshell:

Minimum mountings are:

- 1 for each leg of the main or lateral rollbar ;
- 1 for each of the front rollbar ;
- 1 for each backstay.

Each mounting foot of the front, main and lateral rollbars must include a reinforcement plate, of a thickness of at least 3 mm which must not be less than that of the tube onto which it is welded.

Each mounting foot must be attached by at least three bolts on a steel reinforcement plate at least 3 mm thick and of at least 120 cm² area which is welded to the bodyshell. Examples are shown in drawings 253-18 to 253-24. This does not necessarily apply to backstays (see 29.1.10).

These are minimum requirements. In addition to these requirements, more fasteners may be used, the rollbar legs may be welded to reinforcement plates, the rollcage may be welded to the bodyshell. Rollbar mounting feet must not be welded directly to the bodyshell without a reinforcement plate. If the mounting foot reinforcement plate is welded to the bodyshell, then bolts are not required.

- 29.1.15 Required Reinforcement

- 29.1.15.1 Bends and joints:

Gussets or corner braces joining the front continuous tubing, whether front rollbar or lateral rollbar, and the brace tubing are required. The gusset or brace shall span the weld that joins the two tubes and may be positioned either in the roof quadrangle or in the windshield area.

If gussets are used they must be of a thickness equal to the wall thickness of the roll cage tubing and must be a minimum of 5 cm long where they contact the roll cage tubing. They must be welded along this entire length and relieved in the corners so as to not interfere with existing welds.

If reinforcing tubes are used they must span at least 13 cm across the corner at its longest point but must not be more than half way down or along the members to which they are attached, except for those of the junction of the front rollbar, which may join the junction of the door strut/front rollbar.

- 29.1.16 Optional Reinforcement

The diameter, thickness and material of reinforcements shall be as defined in 29.1.13.

They shall be either welded in position or installed by means of demountable joints.

- 29.1.16.1 Rollcage reinforcement:

It is permitted to reinforce the junction of the main rollbar or the front rollbar with the longitudinal struts (drawings 253-10 and 253-16), as well as the top rear bends of the lateral rollbars and the junction between the main rollbar and the backstays.

A reinforcement as in drawing 253-17B may be added on each side of the front rollbar between the upper corner of the windscreen and the base of this rollbar.

- 29.1.16.2 Roof reinforcement:

Reinforcing the upper part of the rollcage by adding members as shown in drawings 253-9 and 253-9A is permitted.

29.1.16.3 Transverse reinforcing members:

The fitting of two transverse members as shown in drawing 253-7 is permitted. The transverse member fixed to the front rollbar must not encroach upon the space reserved for the occupants. It must be placed as high as possible but its lower edge must not be higher than the top of the dashboard.

29.1.17 Minimum Door Aperture

For all vehicles with a log book issued after January 1, 2002, the presence of the rollcage in the door aperture must comply with the following criteria (see drawing 253-17D):

- dimension A must be a minimum of 300 mm
- dimension B must be a maximum of 250 mm
- dimension C must be a maximum of 300 mm
- dimension D (measured from the upper corner of the windscreen, without the seal) must be a maximum of 100 mm
- dimension E must not be more than the half height of the door aperture.

29.1.18 Minimum Windscreen Aperture

For all vehicles with a log book issued after January 1, 2003, in frontal projection, reinforcements of bends and junctions of the upper corners of the front roll-cage must be only visible through the area of the windscreen described by drawing 253-17E.

29.1.19 Removable members:

Should removable members be used in the construction of a rollcage, the demountable joints used must comply with a type approved by the FIA (see drawings 253-27 to 253-36). They must not be welded.

Where nuts and bolts are used, they must be of at least M8 size of ISO standard 8.8 or better.

Fasteners must be self-locking or fitted with lock washers.

It should be noted that demountable joints must not be used as part of a main, front or lateral rollbar because they act as hinges in the principal structure and allow deformation. Their use is solely for attaching members to the rollbars and for attaching a lateral rollbar to a main rollbar (drawing 253-4). In this last case, hinged joints such as illustrated in drawings 253-30, 253-33 and 253-36 must not be used.

29.1.20 Guidance on welding:

All welding must be of the highest possible quality with full penetration and preferably using a gas shielded arc. They must be carried out along the whole perimeter of the tube. Although good external appearance of a weld does not necessarily guarantee its quality, poor looking welds are never a sign of good workmanship.

When using heat-treated steel the special instructions of the manufacturers must be followed (special electrodes, gas protected welding).

It must be emphasised that the use of heat-treated or high carbon steels may cause problems and that bad fabrication may result in a decrease in strength (caused by brittle heat-affected zones) or inadequate ductility.

29.1.21 Other roll over protection designs.

29.1.21.1 Roll over protection of alternate material or design may be accepted by the scrutineer provided the entrant can produce a certificate specifying the following: the quality of steel used, the dimensions of the tubes, the optional reinforcing members and the mounting to the vehicle, and that the construction is certified to withstand the stress minima given hereafter in any combination on top of the safety cage:

- 1.5 W* lateral;
- 5.5 W fore and aft;
- 7.5 W vertical.

(*W = weight of the car + 150 kg).

The certificate, approved by CARS and signed by qualified technicians, must be presented to the event's scrutineers. It must contain drawings or photos of the safety cage in question including its fixation and particularities, and must declare that the rollcage can resist the forces specified above.

- 29.1.21.2 For vehicles having a log book issued prior to January 1, 2000 and not meeting current regulations may have their roll over protection accepted by the scrutineer provided the safety cage meets the requirements described in appendix 1.
- 29.1.22 FIA homologated rollcages are acceptable provided they are equipped with door bars. It is the entrant's responsibility to present the cage's homologation papers at scrutineering. (Note that the homologation is only valid if the cage is installed exactly according to the manufacturer's instructions, without modification.)

FOR REFERENCE ONLY NOT APPLICABLE FOR NEW VEHICLES

30. ROLLCAGES: UP TO JANUARY 1, 2000

The following rollcage specifications are for reference purposes only for cars with log books issued before January 1, 2000. Please see regulation NRR 12.3.2 for new rollcage requirements.

30.1 Roll Over Protection.

30.1.1 Roll cages are mandatory for all vehicles.

30.1.2 Specific roll over protection is subject to the approval of the scrutineer at each event.

30.1.3 Basic design considerations.

30.1.3.1 The basic purpose of the roll over protection is to protect the crew if the car rolls over or is involved in a serious accident.

30.1.3.2 The roll over protection must be designed to withstand compression forces resulting from the weight of the car coming down on the roll structure and to take fore and aft loads resulting from the car skidding along the ground on the roll structure.

30.1.3.3 The main roll bar hoop must extend the full width of the passenger compartment and must be a minimum of 5 cm above the top of the driver's helmet with the driver sitting in the normal position, or as near the roof as possible in closed cars, and shall not be more than 25 cm behind the back of the driver's helmet. In roll cage construction, a similar hoop must be in front, supporting the front pillars, with horizontal braces connecting the front hoop to the main hoop at each side of the top.

30.1.3.4 It is recommended that any portion of the roll over protection which might be contacted by the driver's or co-driver's helmet be covered with Styrofoam or other energy-absorbing, high-density material to a minimum thickness of 25 mm and this padding be covered by duct tape or similar protective wrapping.

30.1.3.5 For roll cage construction, a lower side bar joining one main hoop and the front hoop on each side of the structure is required. This bar should be installed no higher than one-third of the distance from the floor to the top of the main hoop.

30.1.4 Material.

30.1.4.1 The roll over protection must be of Seamless, ERW, or DOM mild steel tubing, or chrome molybdenum, alloy steel such as SAE 4125 or SAE 4130. It is recommended that mild steel tubing be used as chromium alloys present difficulties in welding and must be normalised to relieve stress. Proof of the use of alloy steel will be the responsibility of the entrant.

30.1.4.2 The size of tubing to be used shall be determined from the following table.

Vehicle Weight	Mild Steel		Alloy Steel	
	Diameter	Wall Thickness	Diameter	Wall Thickness
Under 1500 lb (680 Kg)	1.5" (38mm)	0.095" (2.4mm)	1.375" (35mm)	0.090" (2.3mm)
1500-2500 lb (680-1140 Kg)	1.5" (38mm)	0.120" (3.0mm)	1.5" (38mm)	0.090" (2.3mm)
Over 2500 lb (1140 Kg)	1.75" (44mm)	0.120" (3.0mm)	1.625" (41mm)	0.090" (2.3mm)

An inspection hole of at least 4.5 mm diameter must be drilled in a non-critical area of the main hoop to facilitate verification of wall thickness.

30.1.4.3 Where nuts and bolts are used, the bolts shall be at least 9 mm diameter, SAE grade 5 or equivalent.

30.1.5 Fabrication.

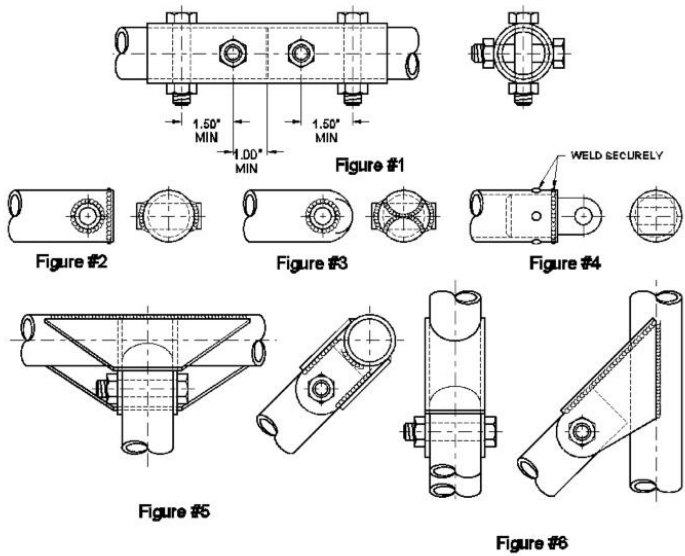
30.1.5.1 One continuous length of tubing must be used for each of the hoops with smooth continuous bends and no evidence of crimping or wall failure.

30.1.5.2 Two rearward facing fore-aft braces must be installed from the main hoop. This bracing must be attached as near as practical to the top of the hoop and at an angle of at least 30 degrees from vertical.

30.1.5.3 If the fore-aft bracing must be removable, the connection between the hoop and the braces must be of the double-lug type, fabricated from material of at least 5 mm thickness and welded through a doubler

or gusset arrangement to avoid distortion or excessive strain caused by welding. Details of the attachment of removable braces follow.

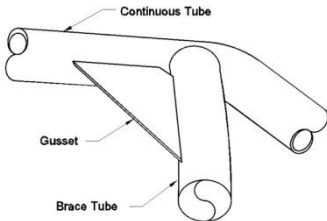
- 30.1.5.4 The fore-aft braces, if removable, should be attached to a rear chassis member through a double-lug connection as described above. If attached to the engine, it must mount to a major component such as a head stud.
- 30.1.5.5 If one tube fits inside another tube to facilitate removal, the outer sleeve must fit tightly and the inner tubes must bottom together (see Figure #1). At least two bolts must be used at each end to secure such a joint and the telescope section must be be at least 20 cm in length.



- 30.1.6 Mounting plates.
- 30.1.6.1 In cars with frame-type construction, the roll over protection must be attached to the frame wherever possible. Mounting plates bolted or welded to the frame must be at least 5 mm thick.
- 30.1.6.2 In cars with unitised or frameless construction, or cars with frames where frame mounting the roll over protection is impractical, mounting plates must be used to secure the structure to the floor of the car. The important consideration is that the loads be distributed over as large an area as possible.
- 30.1.6.3 Mounting plates bolted to the structure shall not be less than 5 mm thick, with a backup plate of equal size on the opposite side of the panel with the plates through-bolted together.
- 30.1.6.4 Mounting plates welded to the structure shall not be less than 2 mm thick. Wherever possible the mounting plate should extend onto a vertical section of the structure such as a door pillar.
- 30.1.6.5 The important consideration is that the load be distributed over as large an area as possible. Mounting plates shall have a minimum area of 16 square inches or 100cm².
- 30.1.7 Gussets

Gussets or corner braces joining the front continuous tubing, whether "front hoop" or "side hoop", and the brace tubing are required. The gusset or brace shall span the weld that joins the two tubes and may be positioned either in the roof quadrangle or in the windshield area.

If gussets are used they must be of a thickness equal to the wall thickness of the roll cage tubing and must be a minimum of 5 cm long where they contact the roll cage tubing. They must be welded along this entire length and relieved in the corners so as not to interfere with existing



welds.

If corner braces are used they must be of tubing a minimum of 2 cm in diameter and 0.060" (1.524 mm) wall thickness. It must span at least 13 cm across the corner at its longest point.

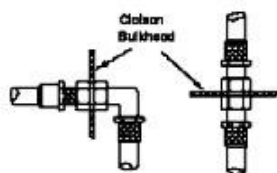
In either case gussets may be fitted parallel to the roof or parallel to the windshield.

30.1.8 Other roll over protection designs.

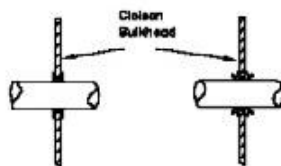
30.1.8.1 Roll over protection of alternate material or design may be accepted by the scrutineer provided the entrant can produce a certificate complying with the following requirements:

- (a) The certificate must specify that the construction is capable of withstanding three simultaneously applied loads:
 - 1.5 g lateral
 - 5.5 g fore/aft
 - 7.5 g vertical
- (b) The induced loads are to be carried over into the primary structure. The mass of the car to be used in these calculations is its mass in starting order with the driver and co-driver aboard and full fuel tanks.
- (c) The certificate must be accompanied by a drawing or a photograph of the roll cage.
- (d) The certificate must be signed by a qualified technician.

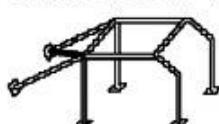
30.1.8.2 FIA roll cages are acceptable. See FIA regulations.



Dessin / Drawing N° 253-1



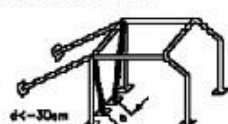
Dessin / Drawing N° 253-2



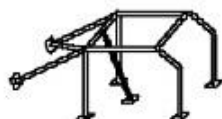
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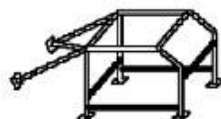
Dessin / Drawing N° 253-4



Dessin / Drawing N° 253-5



Dessin / Drawing N° 253-6



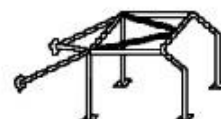
Dessin / Drawing N° 253-7



Dessin / Drawing N° 253-8



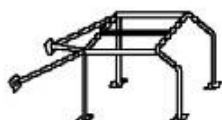
Dessin / Drawing N° 253-9A



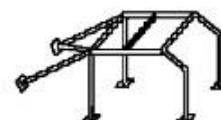
Dessin / Drawing N° 253-9B



Dessin / Drawing N° 253-9C



Dessin / Drawing N° 253-9D



Dessin / Drawing N° 253-9E



Dessin / Drawing N° 253-10



Dessin / Drawing N° 253-11

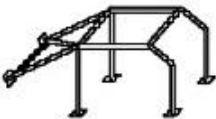


Dessin / Drawing N° 253-12

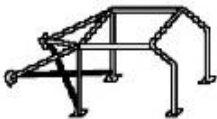


Dessin / Drawing N° 253-13

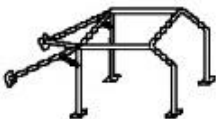
Cette connexion peut être située au niveau de l'entretoise de pontons
This connection may be situated at the level of the dierbar



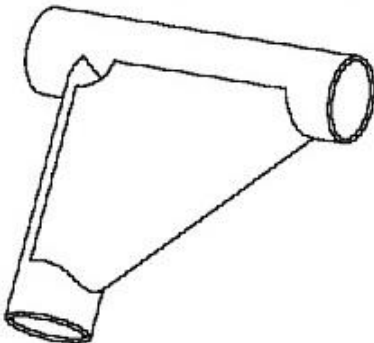
Dessin / Drawing N°253-14



Dessin / Drawing N°253-15



Dessin / Drawing N°253-16



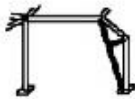
Dessin / Drawing N°253-16A



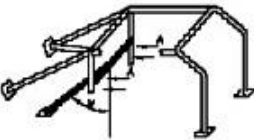
Dessin / Drawing N°253-17



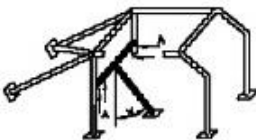
Dessin / Drawing N°253-17A



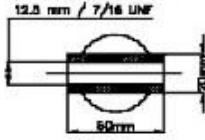
Dessin / Drawing N°253-17B



⊗ Traces de montage pour harnais
Mounting holes for harnesses

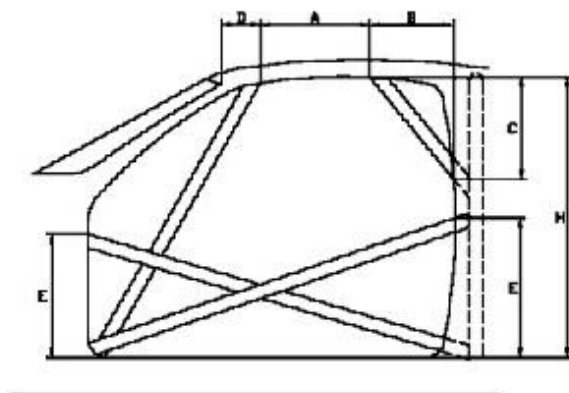


⊗ Angle minimum 30°
Minimum angle 30°

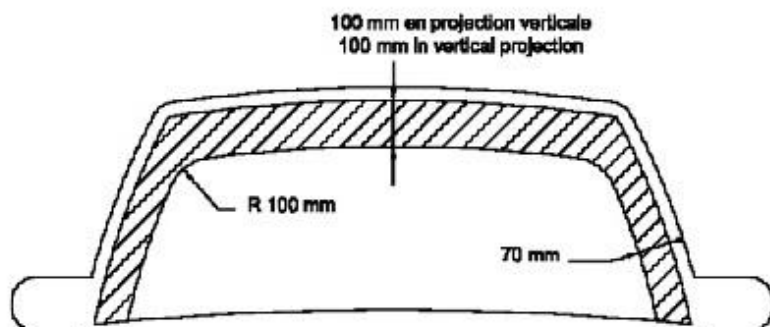


Agrandissement de A
Magnification of A

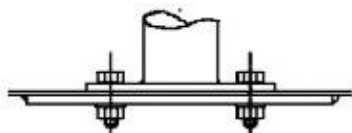
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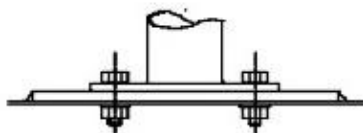
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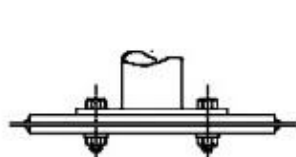
Dessin / Drawing N° 253-17E



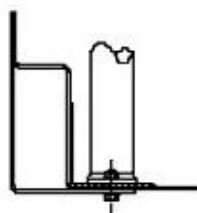
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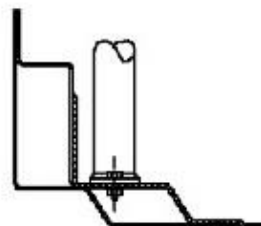
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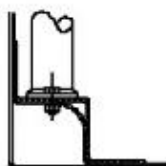
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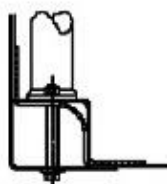
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Dessin/Drawing N°253-22



Dessin/Drawing N°253-23



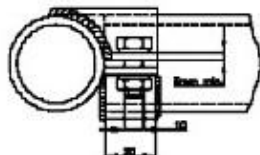
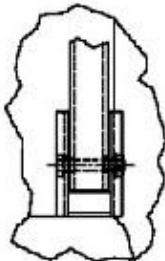
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Dessin/Drawing N°253-25



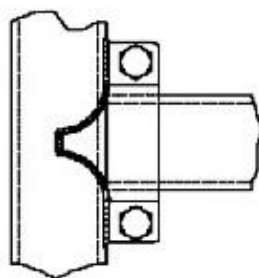
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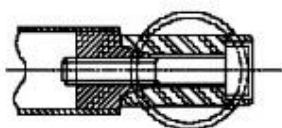


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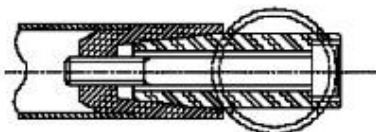


Direction d'application de la charge
Direction of applied load

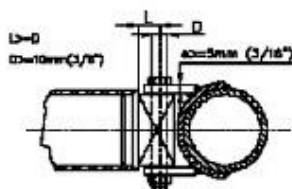




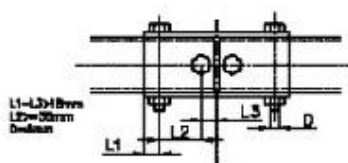
Dessin / Drawing N°253-28



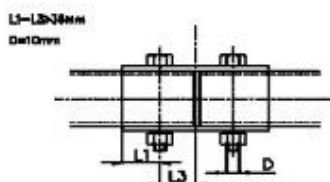
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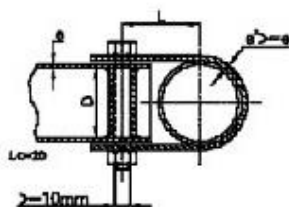
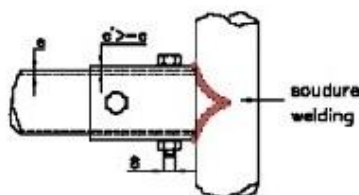
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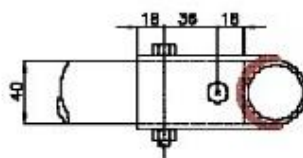
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Dessin / Drawing N°253-32



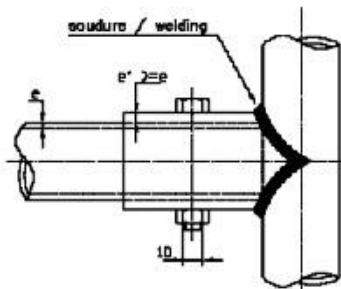
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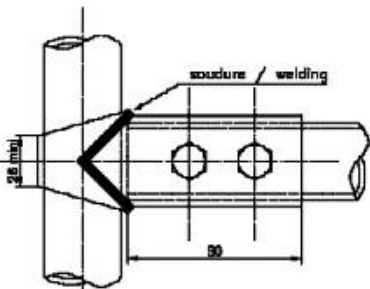
Dessin / Drawing N°253-34
(Dimensions en/in mm)

L doit être minimum
La largeur de la patte doit
être d'au moins 25mm

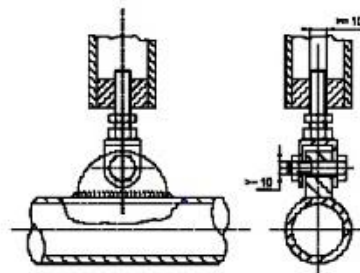
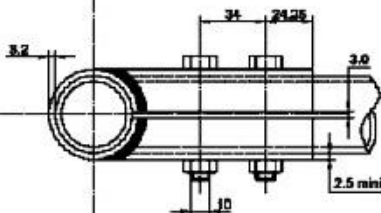
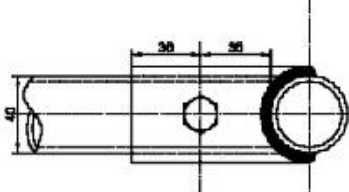
L must be minimum
The sleep width must
be at least 25mm



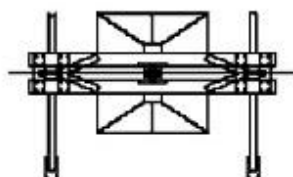
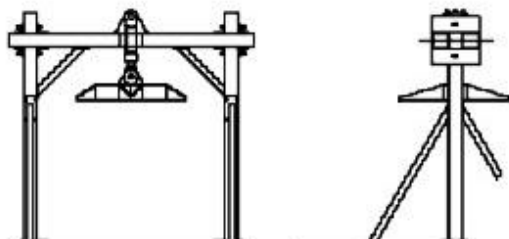
Dessin / Drawing N° 253-35



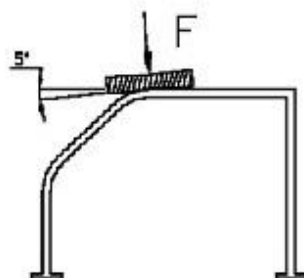
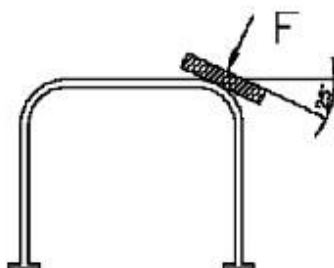
Dessin / Drawing N° 253-36



Dessin / Drawing N° 253-37



Dessin / Drawing N° 253-38



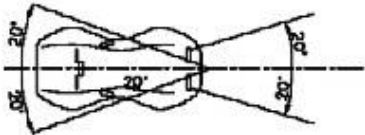
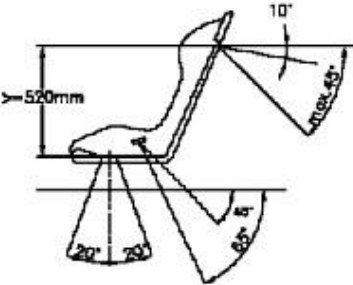
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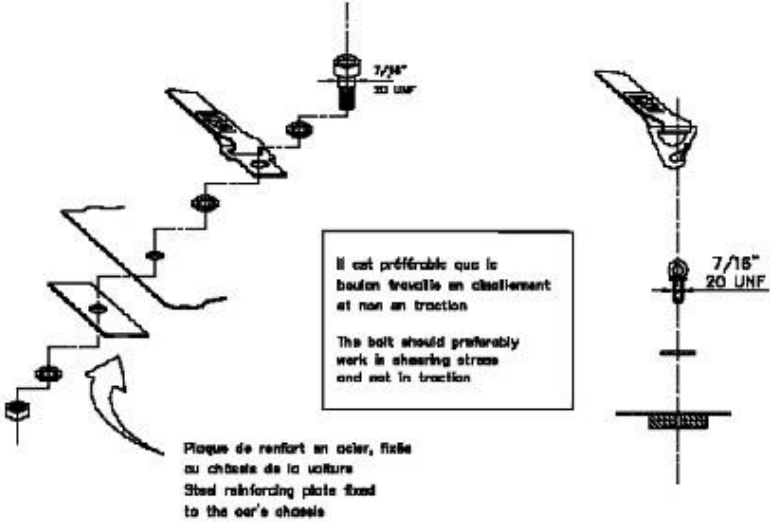
Dessin / Drawing N° 253-39



Dessin / Drawing N° 253-40

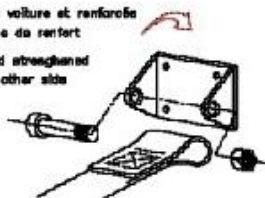


Dessin / Drawing N° 253-42



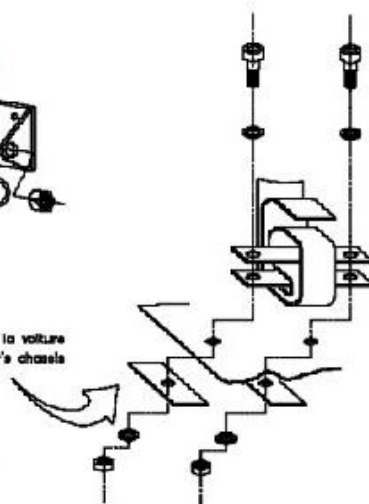
Dessin / Drawing N° 253-43

plaque fixée au châssis de la voiture et renforcée
de l'autre côté par une plaque de renfort
plate fixed to the chassis and strengthened
by a reinforced plate on the other side

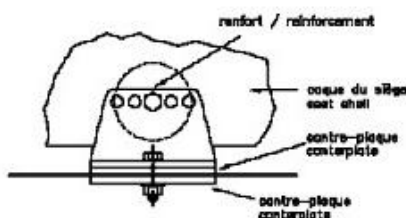


Dessin / Drawing N°253-44

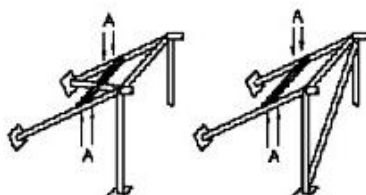
plaque de renfort fixée au châssis de la voiture
reinforcing plate fixed to the car's chassis



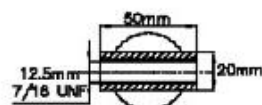
Dessin / Drawing N°253-45



Dessin / Drawing N°253-52



④ trous de montage pour harnais
mounting holes for harness













Agrandissement de A
Magnification of A

Dessin / Drawing N°253-53

1998 Rocky Mountain Rally

page 27

Stage 5: Burke - Beaver

ET/Tempo	Cum	Interval	Instruction	Comments/Remarks
00:20:00	0.00	0.00		Stage Start
	0.10	0.10		Control Zone End
	0.24	0.14		Curve Right - Exposure Left
	1.08	0.84		Texas Gate
	2.12	1.04		Texas Gate
	2.95	0.83	 	Caution! Texas Gate on crest Left over crest
	3.78	0.83		Texas Gate "Bow Crow Forest"
	4.89	1.11		Texas Gate
		0.71		Next Instruction
Organized by the Calgary Sports Car Club				



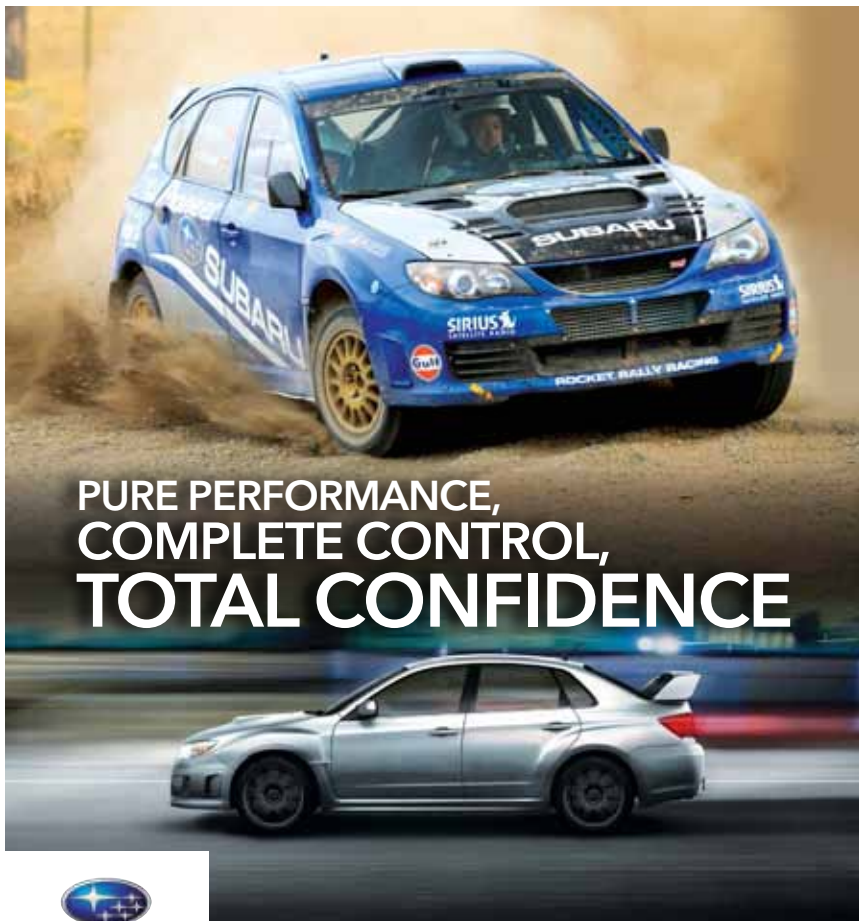
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